

CITY OF PATERSON



PREPARED BY:
HEYER, GRUEL & ASSOCIATES



FOURTH WARD REDEVELOPMENT PLAN

CITY OF PATERSON, NEW JERSEY



FEBRUARY 2017

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INTRODUCTION

PATERSON FOURTH WARD



INTRODUCTION

The City of Paterson's Fourth Ward Redevelopment Area is a 280 acre expanse of land that sits just northeast of the City's Downtown. Situated outside the Downtown, and between two sets of railroad tracks, the Fourth Ward serves as a residential transition area between the Downtown commercial business district, industrial properties along the railroads and the Passaic River, and the more suburban residential neighborhoods on the eastside of the City. The areas of the City in between Ellison Street, Lafayette Street, Straight Street, and Madison Avenue in the Fourth Ward have been plagued by issues of neglect and abandonment for decades. While there has been in recent years some new investment into the community, there is still a great need for revitalization.

The City of Paterson has had a long history of using Redevelopment as a tool for proactively planning the City's future, and stimulating new development in historically underserved areas. Redevelopment as a planning tool for municipalities such as Paterson provides a number of benefits. Redevelopment allows the City to take a more proactive approach to development, and attract the right type of development it seeks to benefit its neighborhoods. Redevelopment also provides financial incentives for potential developers that may not be available otherwise. Developers can qualify for long-term tax abatement and low-interest loans to offset the costs of development, which can help stimulate growth where it otherwise may not occur.

This Plan is an update to the Fourth Ward Redevelopment Plan. Since the original adoption of the Fourth Ward Redevelopment Plan in December

of 2003, the following significant events have taken place which have impacted the Fourth Ward Area:

- **Economic Recession** – The national economic recession that began in 2008 and resulting housing foreclosure crisis had a major impact on the City of Paterson, including many properties in the Fourth Ward. This has contributed to the high number of residences in the Area that are either boarded up or abandoned.
- **Flooding events of Hurricane Irene and Super-Storm Sandy** – Recent storm events that brought heavy rainfall to the region have resulted in devastating floods that left many homes and businesses in the City damaged. Although the majority of the flooding was not within the boundaries of the Fourth Ward Redevelopment Area, the impacts of the storms, such as loss of power, disruptions to the transportation network, and interruptions to the economy, still impacted the Area.
- **Development of a New Master Plan for the City** – The City adopted a new Master Plan in April of 2014, which updated the goals and objectives from the previous Master Plan. The new Master Plan also made specific recommendations for the Fourth Ward Redevelopment Area.
- **Shift in Ward Boundaries** – In 2012, the Ward boundaries of the City were re-drawn. As a result, the boundary between the Fourth and Fifth Wards shifted several blocks south. The previous southern boundary of the Fourth Ward was Broadway. It is now Ellison Street. In order to accurately reflect the current ward



Fourth Ward Redevelopment Area



Fourth Ward Redevelopment Area



Fourth Ward Redevelopment Area



Senior Housing in the Fourth Ward



Wrigley Park



NYS&W Railroad Tracks

boundaries, the Fourth Ward Redevelopment Area has been revised since the 2003 Redevelopment Plan. With the exception of five blocks immediately to the east of Straight Street, which are in the First Ward, the remainder of this Plan covers properties entirely within the Fourth Ward.

- ***Passaic/Bergen Passenger Rail Restoration Project*** – NJ Transit has proposed to utilize an existing New York, Susquehanna, & Western commercial railroad as a passenger service light rail line connecting Hawthorne to Hackensack. These tracks currently run through Paterson, and the initial proposal would include 5 stops within the City, including one near the intersection of Broadway and Madison Avenue.
- ***Madison Avenue Commuter Rail Corridor Study*** – In 2009, a Transit Oriented Development (TOD) study for a potential transit stop near the intersection of Broadway and Madison Avenue was issued by the Passaic County Department of Planning. This report made many recommendations regarding the potential for development that would exist if the proposed light rail were to be installed connecting Hawthorne to Hackensack.

CONTENTS OF A REDEVELOPMENT PLAN

The Redevelopment Plan becomes the formal planning document guiding the redevelopment and revitalization of the Redevelopment Area. The Redevelopment Plan is statutorily required to include an outline for the planning, development, redevelopment or rehabilitation of the Redevelopment Area sufficient to indicate:

1. Its relationship to definitive local objectives as to appropriate land uses, density of population and improved traffic and public transportation, public utilities, recreational and community facilities and other public improvements.
2. Proposed land uses and building requirements in the redevelopment area.
3. Adequate provision for the temporary and permanent relocation, as necessary, of residents in the redevelopment area that will be displaced including an estimate of the extent to which decent, safe and sanitary dwelling units affordable to displaced residents will be available to them in the existing local housing market.
4. An identification of any property within the Redevelopment Area that is proposed to be acquired in accordance with the Redevelopment Plan.
5. Any significant relationship of the Redevelopment Plan to:
 - a. The Master Plans of contiguous municipalities;
 - b. The Master Plan of the County in which the municipality is located; and
 - c. The State Development and Redevelopment Plan adopted pursuant to the “State Planning Act” P.L. 1985,C.398 (N.J.S.A.52:18A-196 et seq.).
6. As of the date of the adoption of the resolution finding the area to be in need of redevelopment, an inventory of all housing units affordable to low and moderate income households, as defined pursuant to section 4 of P.L. 1985 c.222 (C.52:27D-304), that are to be removed as a result of implementation of the redevelopment plan, whether as a result of subsidies or market conditions listed by affordability level, number of bedrooms, and tenure.
7. A plan for the provision, through new construction or substantial rehabilitation of one comparable, affordable replacement housing unit for each affordable housing unit that has been occupied at any time within the last 18 months, that is subject to affordability controls and that is identified as to be removed as a result of implementation of the redevelopment plan.

This Redevelopment Plan provides a detailed guide for the revitalization of this Area of Paterson. The following is a review of the Area, the basis for the development of the Plan in the Master Plan, and the district land use standards. This Plan also provides the administrative procedures and requirements for the implementation of the Plan.

PROCESS

The Fourth Ward Redevelopment Area was originally designated as “an area in need of redevelopment” in the Spring of 2003, along with areas in the First and Fifth Wards of the City. These redevelopment area designations came as a result of the City’s 2003 Master Plan, which recommended that many of the City’s central residential neighborhoods be investigated for their potential to be designated as “areas in need of redevelopment” to help catalyze revitalization. This resulted in the Planning Board undertaking formal redevelopment studies to examine each of these areas, and it was determined at that time that these areas were statutorily in need of redevelopment.

After the redevelopment designation, a formal Redevelopment Plan for the Fourth Ward was adopted by the City Council in December of 2003, which is the document governing land use within the Redevelopment Area.

In 2012, as part of the periodic evaluation of the City’s political boundaries that is mandated every ten years as new census data becomes available, the boundary between the Fourth Ward and Fifth Ward shifted two blocks south from Broadway to Ellison Street. This area, which was previously part of the Fifth Ward Redevelopment Area, has been removed from that plan and added to the Fourth Ward Redevelopment Area to be consistent with the City’s political divisions.

In April of 2014, the City’s Planning Board adopted a new Master Plan document. As a part of the process of developing the Master Plan, public

meetings were held in each Ward of the City, to discuss issues specific to those areas.

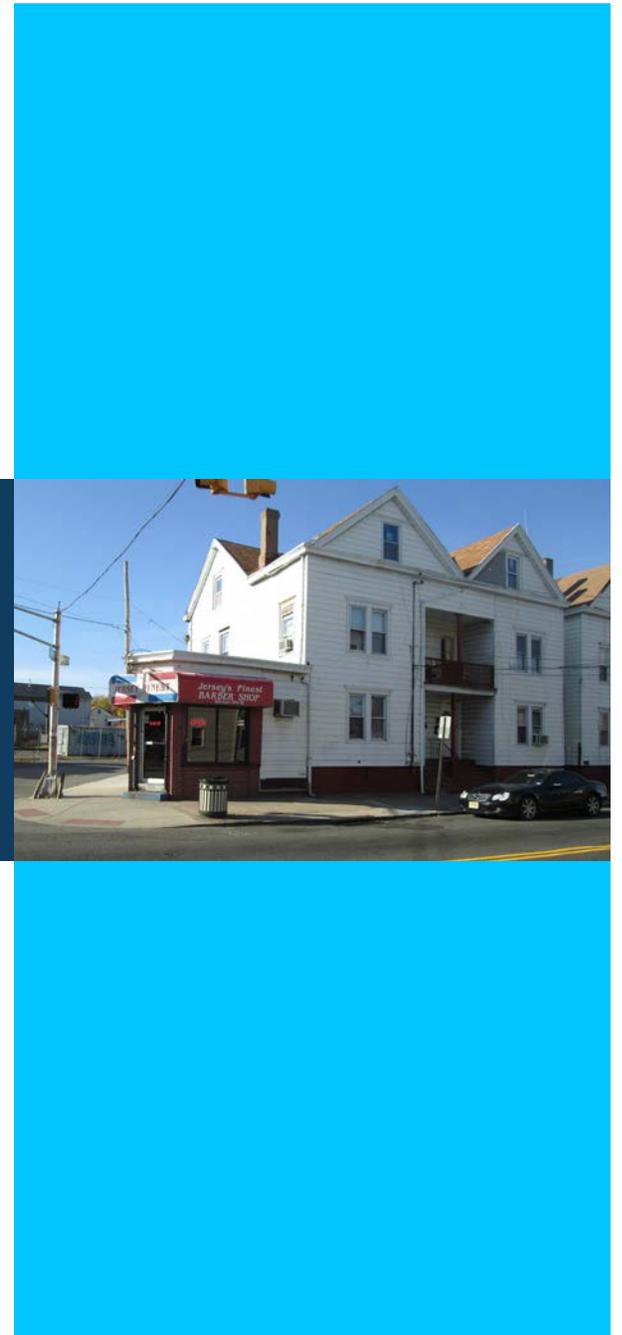
The following key issues were noted regarding the Fourth Ward Redevelopment Area:

- High levels of vacant and abandoned property
- High levels of crime in certain areas
- Too many 99 cent stores
- Too many liquor stores
- Need for affordable housing

This update to the Fourth Ward Redevelopment Plan is a direct output of the 2014 Master Planning process.

PLANNING CONTEXT

PATERSON FOURTH WARD



City of Paterson, NJ

Fourth Ward Redevelopment Plan

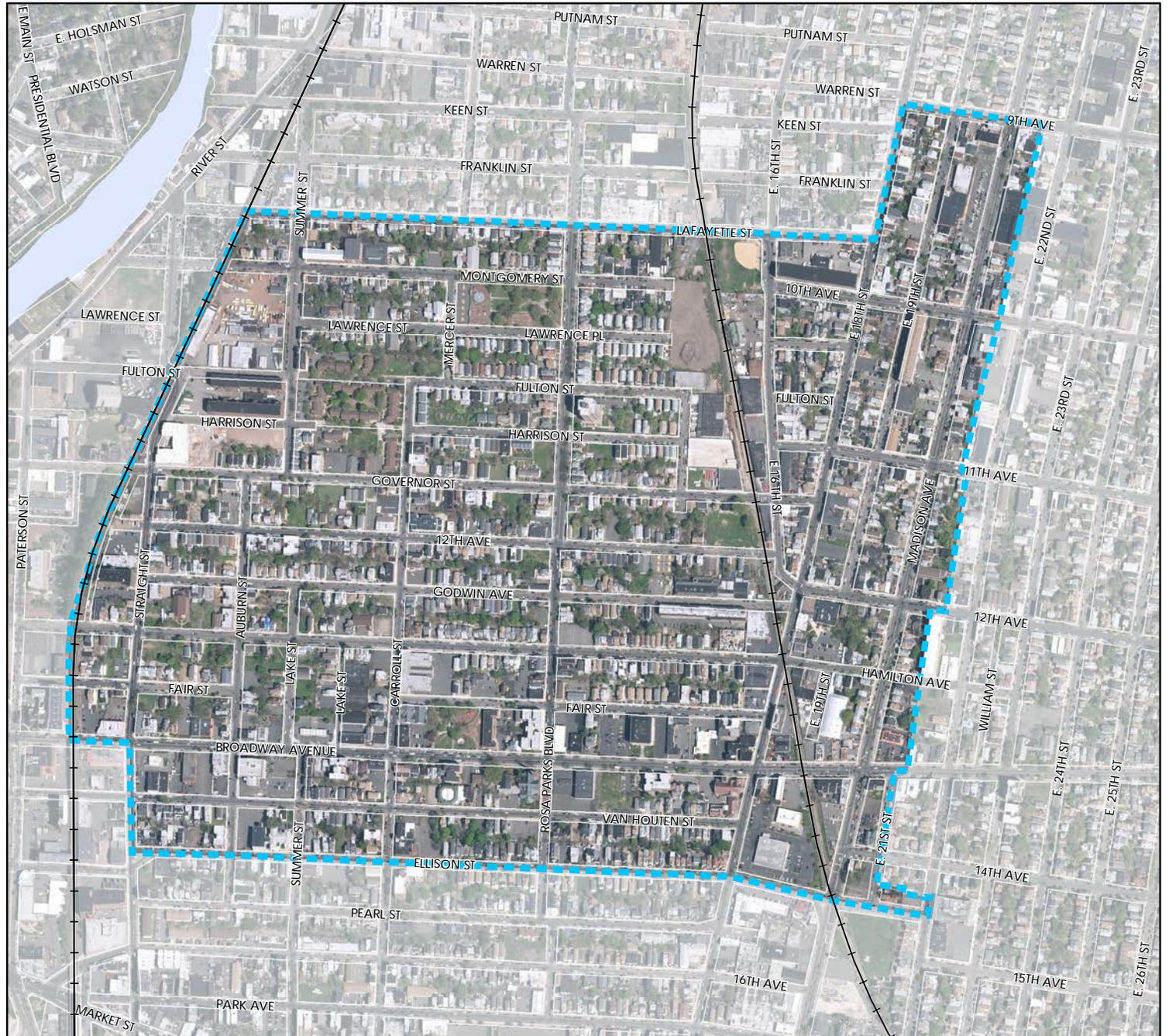
Area Boundaries

 Fourth Ward Redevelopment Area



Source: NJGIN, NJDEP, NJOGIS, NJDOT, City of Paterson, FEMA

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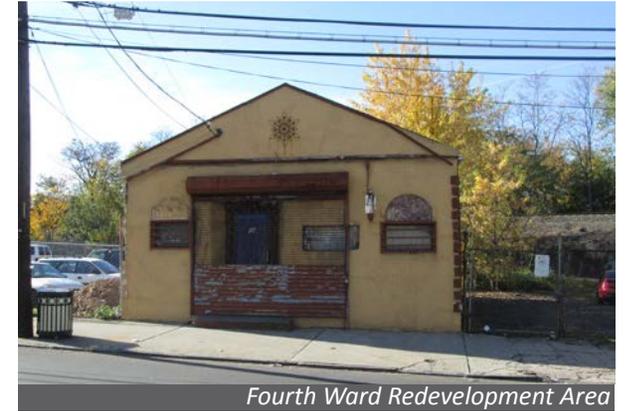
DESCRIPTION OF AREA

Boundary Description

The Fourth Ward Redevelopment Area (the Area) consists of the portions of the City bound by Ellison Street to the south, the New Jersey Transit Railroad tracks to the west, Lafayette Street to the north, and Madison Avenue to the east. The Area in total is 280 acres in size, and is centrally located within the borders of Paterson, just northeast of the Downtown of the City.

Layout and Orientation

The Fourth Ward Redevelopment Area is generally flat terrain, with streets laid-out along a typical urban street grid. The Area is somewhat isolated from other parts of the City in that it is bound both on the west and near its eastern edge by railroad tracks that cut through the street grid, creating a number of dead ends and cutting off the Area from the other sides of the tracks. In addition to the railroads, there are a number of other dead-end streets and “T” intersections throughout the Area. Although generally on a grid, several streets, such as Summer Street, Fulton Street, and Harrison Street, are broken up to create much larger blocks. The effect of these closed streets and the railroad tracks is that there are some very small blocks, and some very large blocks in the Area, and very few streets which provide significant connections to the surrounding City, with the exception of Broadway, which forms a major east-west connection to the central business district of the City.



Fourth Ward Redevelopment Area



Western Boundary of Area



Fourth Ward Redevelopment Area

Land Use

Land Use in the Area

The Fourth Ward Redevelopment Area boundaries have changed since the 2003 Redevelopment Plan was adopted. The Area has expanded slightly as the ward boundary between the 4th and 5th Ward shifted two blocks to the South. An expansion of the Area was also designated as in need of redevelopment in 2016. The Area has been essentially built-out with a mix of industrial, residential, and various commercial uses for decades. Over 40% of the Area is devoted to residential uses, whether they are single-family homes or multi-family apartment buildings. The residences are dispersed throughout the Area, with many of the blocks being lined by two or three story homes. Many of these homes are currently used as two-family or three-family dwellings, although they were likely originally constructed as single-family homes. The levels of residential use in the Area have remained consistent over the past decade.

Commercial properties make up the next most prevalent land use within the Area, taking up 22% of the land area. Although scattered throughout the Area, most of the commercial properties can be found along one of three corridors; Broadway, Madison Avenue, or Rosa Parks Boulevard. These corridors are the primary retail commercial areas of the Fourth Ward. There are several larger commercial properties along the railroad on the east side of the Area, however these are not typical retail or office commercial, but businesses that are more similar to light industrial uses than they are of typical urban commercial stores. The

amount of commercial land use in the Area has increased in the past decade substantially, and has been consistent with a corresponding reduction in vacant land. Many of the commercial properties may also contain residences, as many of the retail stores have apartments above them. However, for the purposes of analyzing tax data, each property is only listed in one land use category, and mixed use is not a classification.

The third largest land use category in the Area is lands used by religious and not-for-profit organizations. There are a number of churches and houses of worship in the Area, and many of them also own and use other properties for a number of purposes. These properties account for nearly

11% of the Area. The remainder of the land in the Area is used primarily by schools, and public entities. Much of the land that is publicly owned is City parks.

There is also a high level of vacant property in the Area, even though the figure has declined significantly in recent years. Vacant land previously accounted for over 13% of the Area, where it now accounts for less than 6%. These vacant properties are undeveloped properties, and do not include properties that are developed with a building, but are not currently occupied. Further discussion of vacant & abandoned properties can be found in the Opportunities for Strategic Development Section of this Plan. Vacant properties are scattered

Fourth Ward Redevelopment Area				
Property Classification	2016		2004	
	Acres	Percentage	Acres	Percentage
Vacant	11.7	5.8%	22.8	13.4%
Residential	72.3	35.9%	57.3	33.7%
Apartment	8.2	4.0%	7.4	4.3%
Commercial	44.2	22.0%	23.4	13.7%
Industrial	12.6	6.3%	14.5	8.5%
Railroad	1.9	1.0%	0.0	0.0%
Public School	3.9	1.9%	2.7	1.6%
Public Property	17.8	8.8%	12.2	7.2%
Church & Charitable	21.7	10.8%	15.0	8.8%
Other Tax Exempt	6.8	3.4%	8.7	5.1%
Unknown	0.5	0.2%	6.2	3.6%
Total:	201.5*	100.0%	170.2	100%

Source: MOD IV Tax Records

*Area does not include roads, and includes a portion of the Area previously in the 5th Ward and the 2016 Expansion. Area including roads is 280 acres.

City of Paterson, NJ
Fourth Ward Redevelopment Plan

Existing Land Use

 Fourth Ward Redevelopment Area

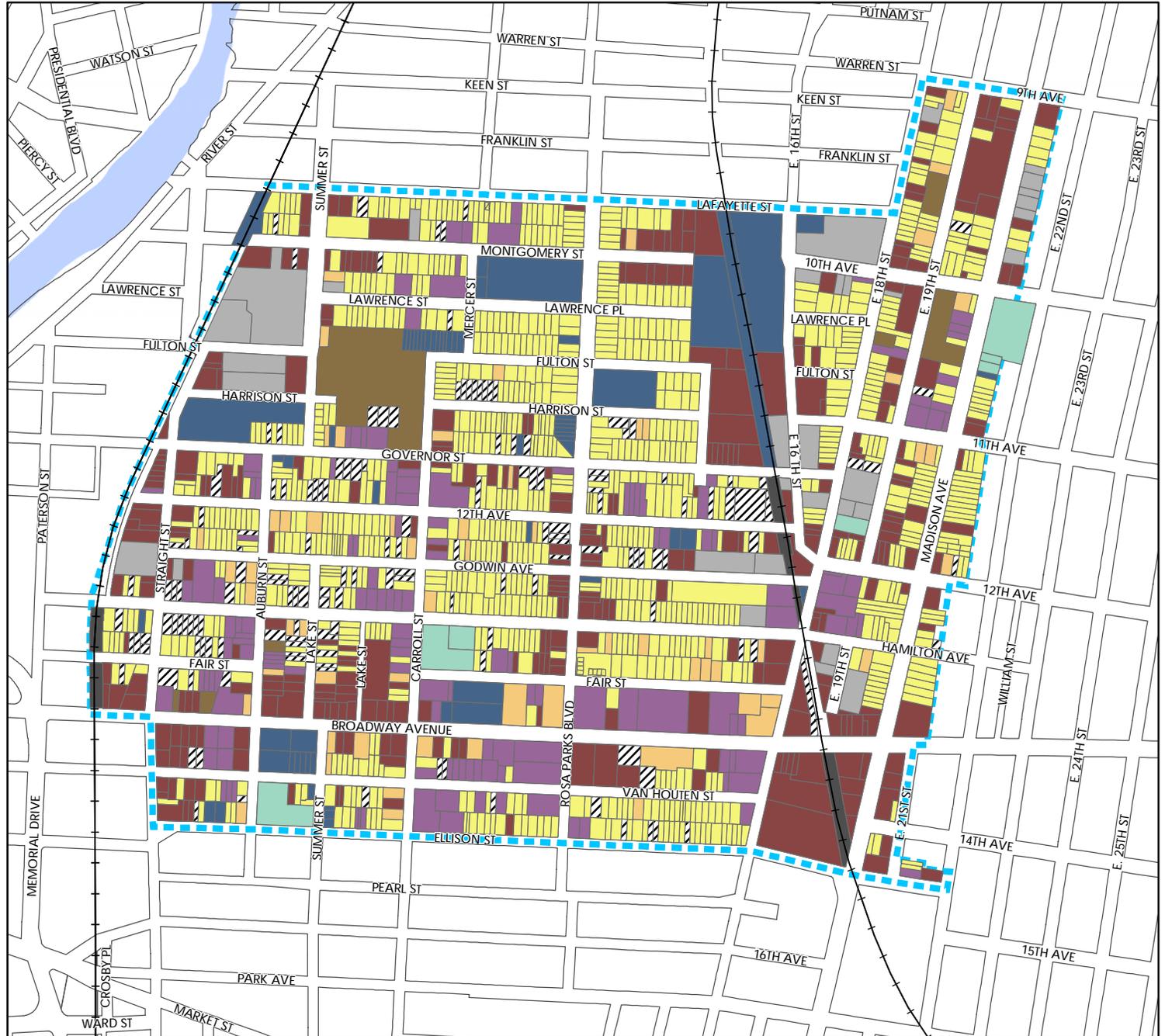
Existing Land Use

-  Vacant
-  Residential
-  Apartment
-  Commercial
-  Industrial
-  Railroad
-  Public School
-  Public Property
-  Church & Charitable
-  Other Exempt



Source: NJGIN, NJDEP, NJOGIS, NJDOT,
City of Paterson, 2014 MODIV Tax
Assessment Data

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City of Paterson, NJ
 Fourth Ward Redevelopment Plan

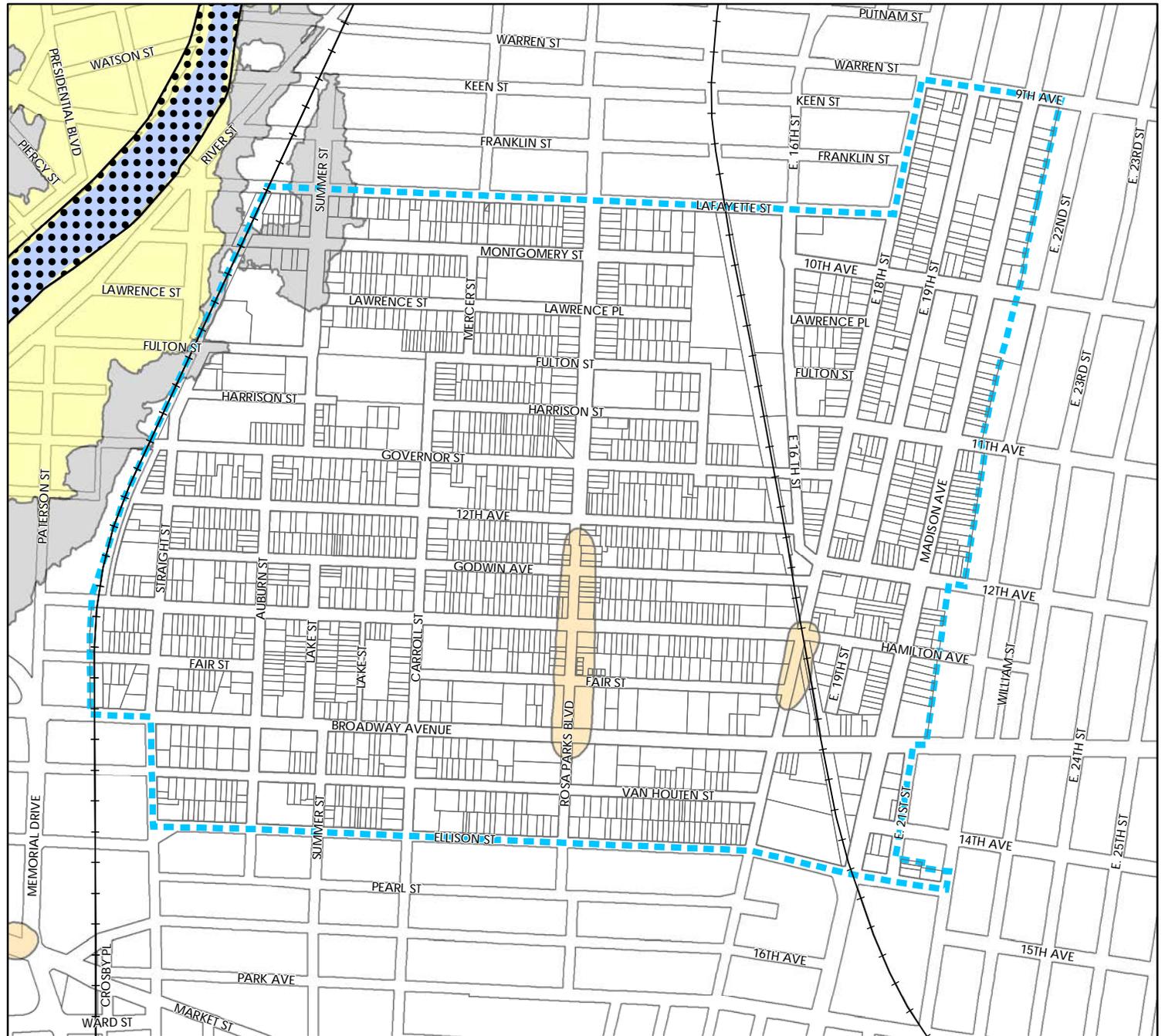
FEMA Floodplains

-  Fourth Ward Redevelopment Area
- FEMA Floodplains (2015)**
-  AE Zone: Areas subject to the 1% annual chance flood (100 year floodplain)
-  AO Zone: Areas subject to the 1% annual chance shallow flooding where depths are between one and three feet. Some areas may have high flood velocities such as alluvial fans and washes.
-  Areas subject to the 0.2% annual chance flood (500-year floodplain)
-  Floodway



Source: NJGIN, NJDEP, NJOGIS, NJDOT, City of Paterson, 2014 MODIV Tax Assessment Data

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throughout the Area, most of which are small residential lots.

Building Conditions

When the Area was designated as an “area in need of redevelopment” in 2003, it was due to a combination of having buildings that were substandard or dilapidated, abandoned, or obsolete for a number of reasons. Throughout much of the Area, the conditions that led to a redevelopment designation remain. While some buildings and properties have been maintained and renovated, a number of other buildings either remain or have fallen into a state of disrepair. These conditions are present throughout the Area, although they are most prevalent in the central blocks along Godwin Avenue, 12th Avenue, and Governor Street.

Flood Hazard Areas

According to data from the Federal Emergency Management Administration (FEMA), very few areas within the Fourth Ward Redevelopment Area fall within either the floodway, or within the ‘100 year floodplain’, which means that there is a 1% chance in any given year that the area will be inundated with flood waters. There are two narrow corridors considered to be within this floodplain in the Area, Rosa Parks Boulevard near Broadway, and 18th Street near the railroad tracks. These areas are outside of the Passaic River floodplain, and flooding in these isolated sections would be due to rainwater backing up, and not a cresting of the river. The northwest corner of the Area, along Summer Street is located within the ‘500 year floodplain’, meaning it is classified as having a 0.2% chance of flooding in any given year. This section



Abandoned Building



Abandoned Building



Vacant Property



Industrial Property



Abandoned Housing



Under-Utilized Commercial Property



Wrigley Park



Pre-K Center



Riverside Fire Station

of the Area is the closest to the Passaic River, and is potentially subject to flooding from the River during large storm events.

COMMUNITY FACILITIES

Community Resources in the Area

There are two public schools that are located within the Fourth Ward Redevelopment Area. Both of these schools are elementary/intermediate level schools. The City’s high schools are located in other neighborhoods outside of the Area.

Schools in the Area:

- E.W. Kilpatrick School – 295-315 Ellison Street (grades K-5)
- Public School # 6 – 137 Carroll Street (grades K-8)
- Public School # 21 – 322 Tenth Avenue (grades K-8)

In addition to the public schools serving the Area, there are several other community facilities that provide services to residents. These facilities include:

- Paterson Community Health Center (227 Broadway)
- Riverside Fire Station (236 Lafayette Street)
- Danforth Public Library

There are also a number of non-profit, religious, and faith based organizations and houses of worship located throughout the Area. These include:

- Christian Fellowship Center and Outreach Ministries
- United Presbyterian Church
- Paterson YWCA

Parks and Open Space

There are relatively few parks and open space areas within the Fourth Ward Redevelopment Area. Barbour Park, Wrigley Park (also known as Rigley’s Park or Montgomery Park), and the 12th Avenue Playground are typical residential urban parks, occupying small areas within a block, and containing mostly areas for passive recreation or a few basketball courts. There is also a baseball field, known as Putnam’s Oval, located on City owned property on the corner of Lafayette Street and 16th Street in the northeast corner of the Area.

Historic Resources

There are several identified historic resources within the First Ward Redevelopment Area.

- Bethel AME Church – (24 Auburn Street) This church is noted for hosting a speech by Dr. Martin Luther King Jr. during his stay in Paterson, just 8 days prior to his murder.
- Masonic Temple – (385 Broadway) The Masonic Temple on Broadway is noted as being a landmark building for its significant mass, and the Renaissance Revival style of architecture which it displays. The Temple was originally constructed in 1923 and designed by local architect Fred Wesley Wentworth.
- I.A. Hall Mills – (94 Fulton Street) The Hall

City of Paterson, NJ
Fourth Ward Redevelopment Plan

Parks, Open Space & Schools

- Parks/Open Space
- Educational Facilities
- Public School
- Preschool
- Fourth Ward Redevelopment Area



Source: NJGIN, NJDEP, NJOGIS, NJDOT, City of Paterson

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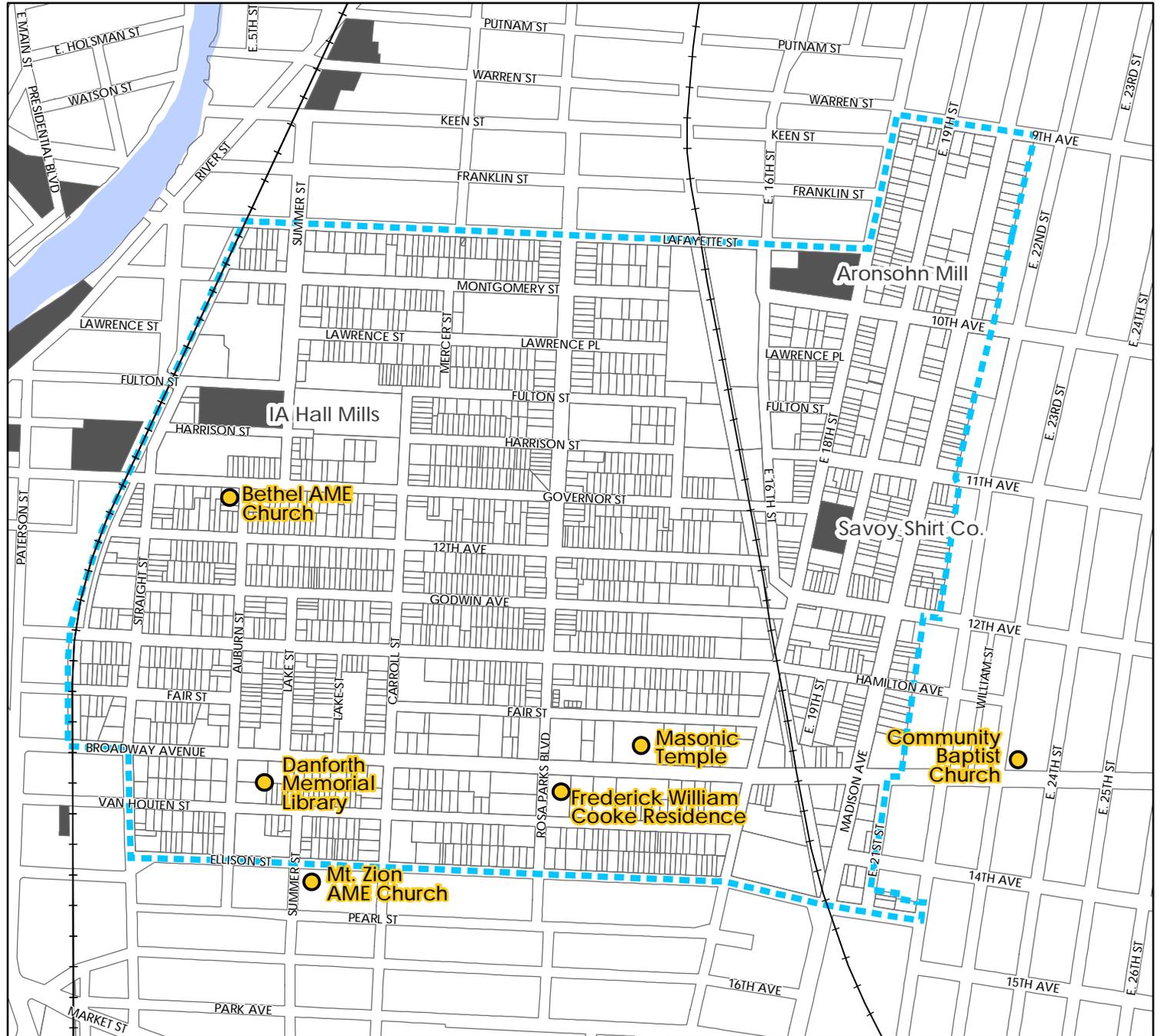
City of Paterson, NJ
 Fourth Ward Redevelopment Plan

Historic Places & Districts

-  Historic Places
-  Historic Mill Survey
-  Fourth Ward Redevelopment Area

Source: NJGIN, NJDEP, NJOGIS, NJDOT, City of Paterson

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Mills are a complex of two 4-story brick mills that are seen as representing turn-of-the-century textile mill construction.

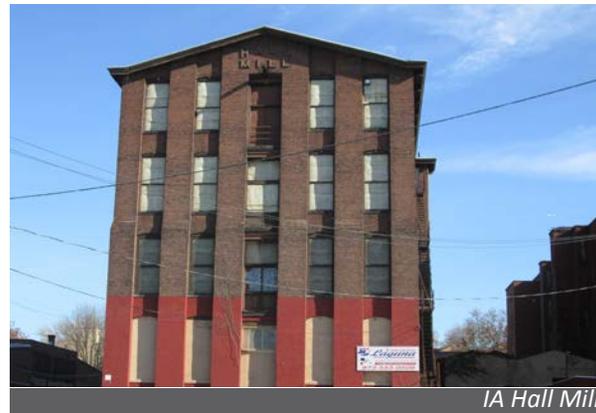
- Savoy Shirt Company – (578 East 19th Street) The Savoy Shirt Company complex is a smaller, two story textile mill that was built to fit into its residential context. Constructed between 1880 and 1910, the ‘U’ shaped structure was used as a garment and silk works.
- S. Aronsohn Mill – (245 10th Avenue) The Aronsohn Mill is a facility of two brick structures originally constructed in 1908 as a silk manufacturing mill. The mill is noted as being exemplary of classic textile mill architecture.
- Danforth Memorial Library – (250 Broadway)
- Frederick William Cooke Residence – (Broadway) Since demolished
- St. Paul’s Episcopal Church – (Broadway)
- Barbour Park Historic District (aka Fair Street District) – Although not currently designated as a historic district, this area is noted as being eligible for historic designation. The area encompasses several blocks surrounding Barbour Park on Broadway.



Bethel AME Church



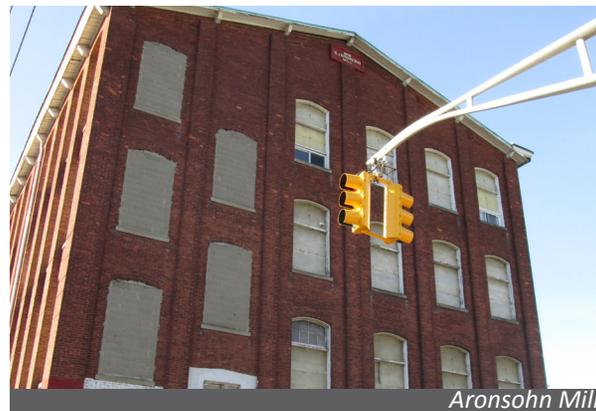
Masonic Temple



IA Hall Mill



IA Hall Mill



Aronsohn Mill



Barbour Park

City of Paterson, NJ

Fourth Ward Redevelopment Plan

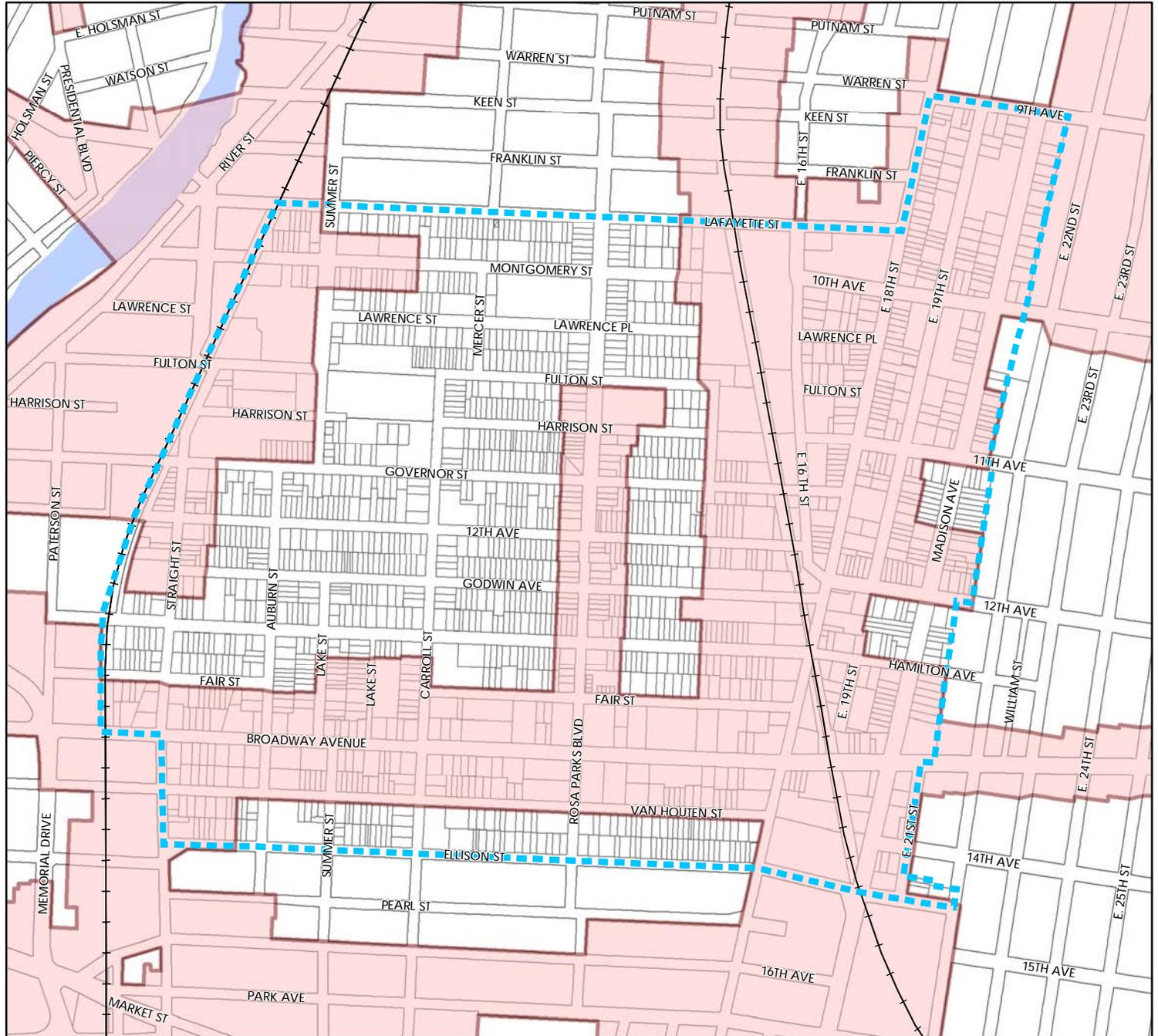
Urban Enterprise Zone

- Urban Enterprise Zone
- Fourth Ward Redevelopment Area



Source: NJGIN, NJDEP, NJOGIS, NJDOT, City of Paterson

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URBAN ENTERPRISE ZONE

The Urban Enterprise Zone is a state program designed to help encourage employment and economic development in urban areas through incentives such as a reduced sales tax for consumers, and subsidized unemployment insurance for employers. In designated Urban Enterprise Zones, sales tax is reduced to 3.5% rather than 7%. UEZ member businesses also receive tax exemption on many operating expenses incurred. The UEZ also helps to administer and coordinate improvements to business areas as well as assist with small business loans to members.

Within the Fourth Ward Redevelopment Area, approximately 1/3 of the Area is within the designated boundaries of the Urban Enterprise Zone. All of Broadway is within the UEZ, and the majority of Rosa Parks Boulevard as well. Most of the eastern portions of the Area are within the UEZ as well.



Business in the Fourth Ward



Business in the Fourth Ward



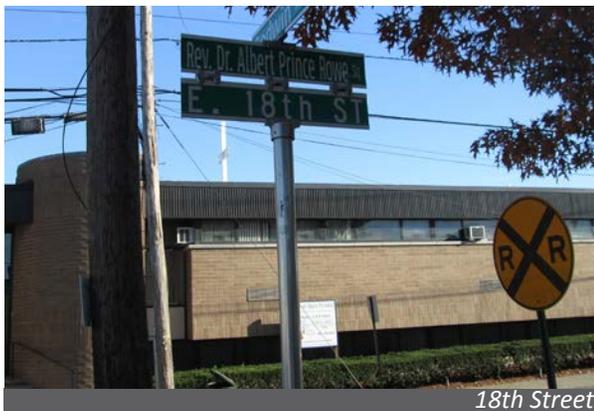
Business in the Fourth Ward



Rosa Parks Boulevard



Broadway at Straight Street



18th Street

TRANSPORTATION

Circulation in the Area

The primary arterial streets in the Area are:

- Broadway
- Lafayette Street (Passaic County Route 650)
- Madison Avenue (Passaic County Route 649)
- Rosa Parks Boulevard
- 10th Avenue (Passaic County Route 651)
- Straight Street (Passaic County Route 647)

The Fourth Ward Redevelopment Area is made up of streets that all are on a traditional street grid, where every street runs either north/south or east/west. No streets within the Area deviate substantially from this grid pattern. However most of the streets in the Area, with the exception of the primary arterial streets noted above, do not extend through the Area. The majority of the streets in the Area either lead to a dead-end at the railroad tracks, or are cut-off at an intersection with another perpendicular street. This creates a hierarchy of streets where those thoroughfares that do extend through the Area become primary collectors and the most frequently used.

The major east/west thoroughfares in the Area are Lafayette Street, which forms the northern boundary of the Area, and Broadway, which runs through the southern portion of the Area. Lafayette Street connects the bridge over the Passaic River at Haledon Avenue, east across both sets of railroad tracks, while Broadway connects

Downtown Paterson with areas east, such as Eastside Park, and eventually across the Passaic River to Elmwood Park. Both Straight Street and Madison Avenue run north/south, and generally form the eastern and western edges of the Area. Straight Street connects the northern portions of the City with the Downtown and South Paterson, while Madison Avenue connects the northern tip of the City with the southern end of the City. Rosa Parks Boulevard is a north/south road which bisects the Area, connecting the northern end of the Area to the southern end.

City of Paterson, NJ
Fourth Ward Redevelopment Plan

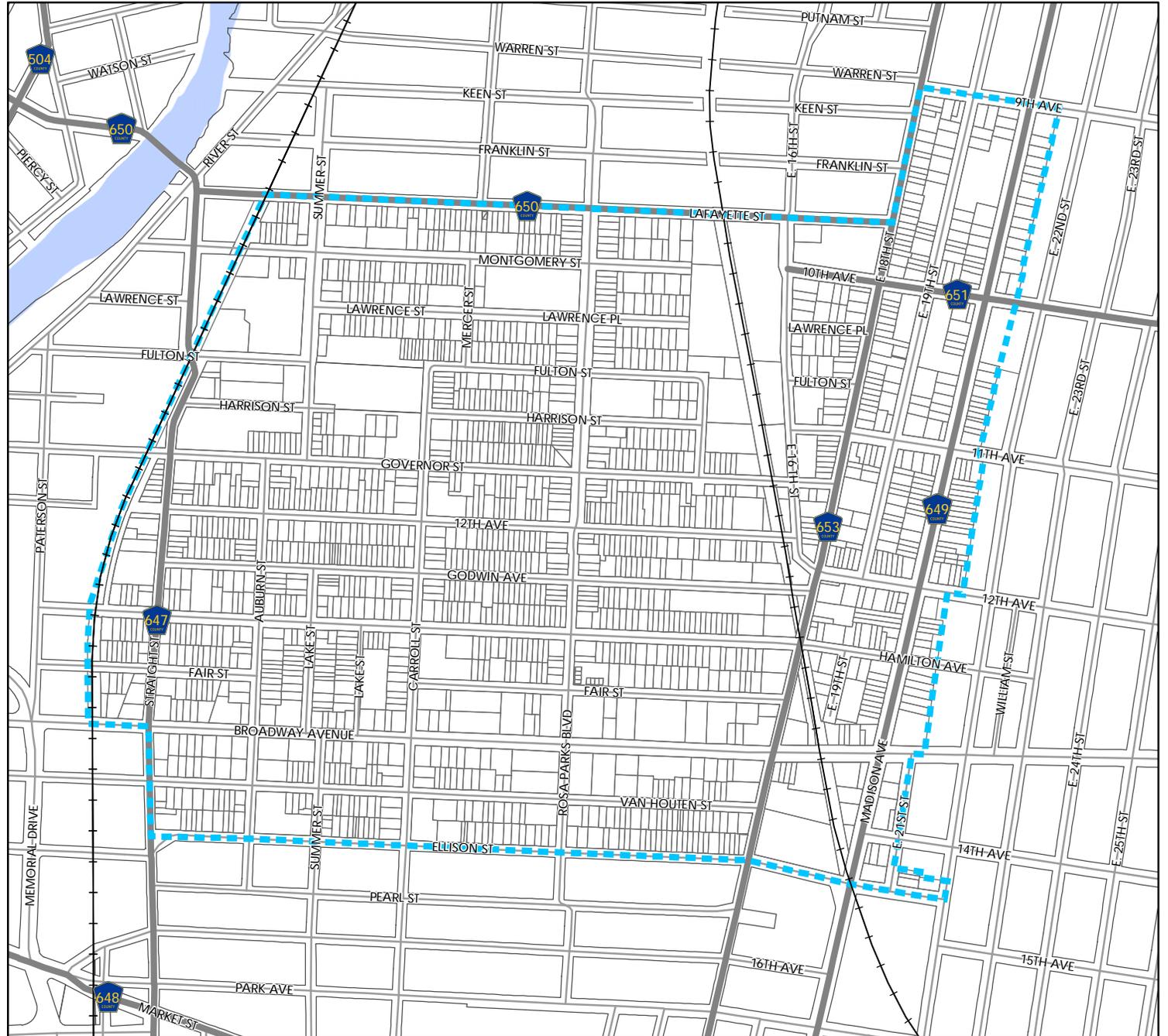
Street Network

-  Road Classification
-  County Route
-  Local Roads



Source: NJGIN, NJDEP, NJOGIS, NJDOT,
City of Paterson

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Fourth Ward Redevelopment Plan

Public Transportation

Bus Stops

- 171
- 365
- 704
- 748
- 770
- Other Bus Stops

Bus Routes

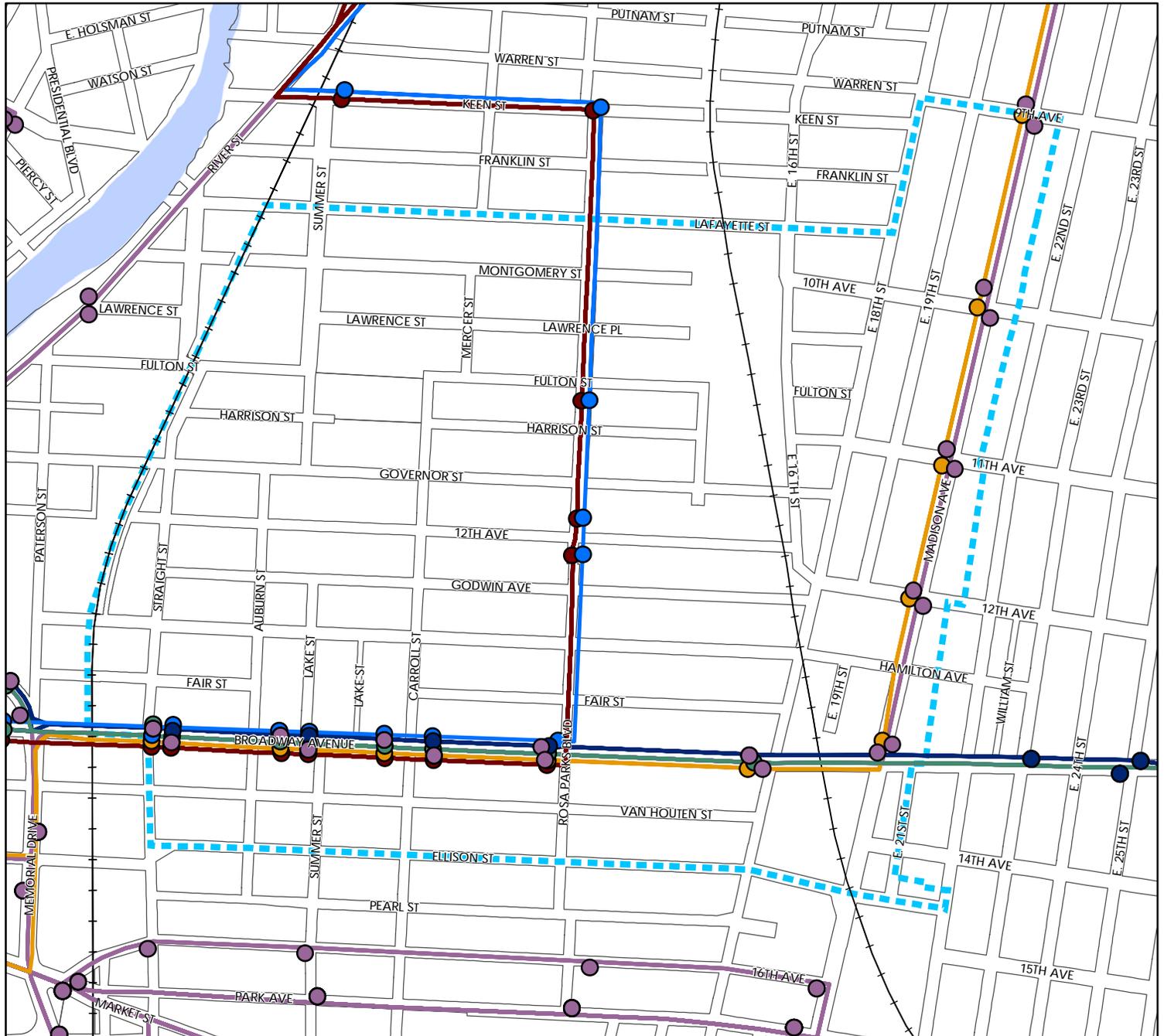
- 171
- 365
- 704
- 748
- 770
- Other Bus Routes

- - - Fourth Ward
Redevelopment Area



Source: NJGIN, NJDEP, NJOGIS, NJDOT,
City of Paterson

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Public Transportation in the Area

There are many NJ Transit bus routes that provide service within the Fourth Ward Redevelopment Area.

- Route 171 – This route connects Paterson via the Broadway Bus Terminal to the George Washington Bridge Bus Terminal in New York City. This route makes several stops along Broadway, as well as points in Fair Lawn, Paramus, Fort Lee, and Teaneck.
- Route 704 – This route connects Paterson to the Willowbrook Mall in Wayne, and stops in the Fourth Ward Area along Rosa Parks Boulevard and Broadway. Other stops along this route include points in Downtown Paterson, Woodland Park, and Little Falls.
- Route 746 – Route 746 runs between Ridgewood and Paterson, beginning at the Broadway Bus Terminal in Paterson, and running to the Ridgewood Bus Terminal at Van Neste Square. This route makes stops on Broadway and Madison Avenue within the Area, and also stops in Fair Lawn and Glen Rock.
- Route 748 – This bus route provides service between Paterson on Madison Avenue in the northern end of the City, to the Willowbrook Mall in Wayne. This route stops along Madison Avenue and Broadway within the Area, and connects with Downtown Paterson, and several other areas of the City to the northwest of Downtown before travelling through Haledon and Pompton Lakes on the way to Wayne.

- Route 770 – This route connects Paterson to Hackensack. The route runs between the Broadway Bus Terminal in Paterson and the Hackensack Bus Terminal, stopping along Broadway in the City, and in areas of Fair Lawn, Paramus, and Rochelle Park.

From Downtown Paterson, either at the Broadway Bus Terminal or at the Ward Street Train Station, there are many other connections throughout the region that can be accessed. NJ Transit Paterson Ward Street train station is approximately 1/2 of a mile from the Fourth Ward Redevelopment Area, and the Broadway Bus Terminal is just west of the Area.



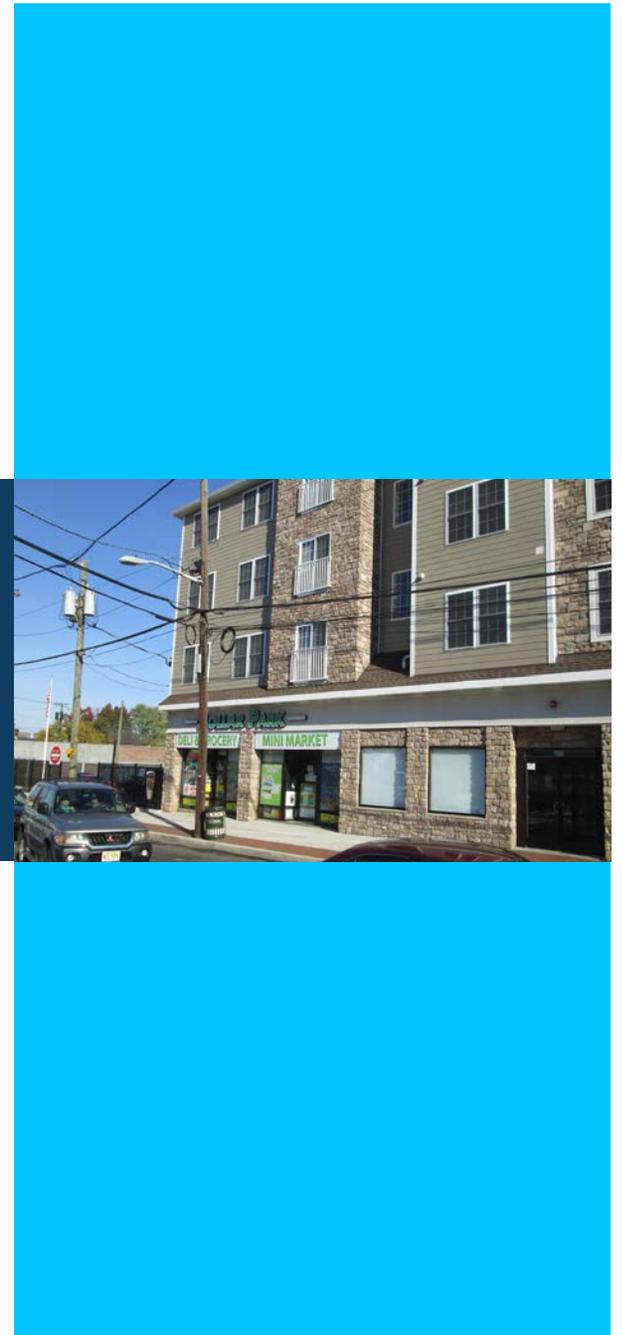
Bus Service in Paterson



Broadway Bus Terminal

GOALS & OBJECTIVES

PATERSON FOURTH WARD



GOALS AND OBJECTIVES

- **Revitalize the Fourth Ward of the City** – The primary goal of this Plan, and the redevelopment effort is to bring economic and community development to the Area, and to improve the quality of life for residents and businesses in the Area.
- **Assemble parcels for larger development projects** – While the redevelopment of the Fourth Ward Redevelopment Area will be mostly infill development, identifying larger properties or tracts of contiguous smaller properties could provide opportunities for more substantial redevelopment projects that could have a greater impact on the Area.
- **Reduce the blighting impacts of abandoned/vacant properties** – The Fourth Ward Redevelopment Area has been impacted tremendously by properties becoming abandoned or left vacant, more so than most other areas of the City. Addressing dilapidated and abandoned buildings can help transform the Area.
- **Provide quality new housing options for Area residents** – Much of the housing in the Area is in poor condition. New housing that caters to a range of potential residents can bring new life to the Area, and provide quality options for current residents looking for better housing.
- **Create cohesive, and vibrant commercial districts to serve the local population** – Area revitalization efforts must include commercial businesses to serve the residential population.

Revitalized neighborhood commercial districts can improve quality of life for local residents, as well as potentially bring in outside investment to the community.

- **Promote mixed use development** – The City should encourage traditional forms of housing and retail, where store owners can have the option of living above their store. Allowing mixed uses in the commercial corridors of the Area can help develop small businesses, as well as provide the mixing of uses and people that help commercial areas succeed.
- **Coordinate the Redevelopment Plan with the Master Plan** – With the newly adopted Master Plan for the City, as well as other plans such as the Madison Avenue TOD study, there is a need to ensure that the Redevelopment Plan for the Fourth Ward is consistent with the other plans for the City, and that there are no discrepancies between documents.
- **Increase Opportunities for Home Ownership** – Current levels of home-ownership within the Area are very low, and higher levels of home ownership can help to bring stability, and connectedness for residents of the community. Home ownership can also help generate wealth for new home owners.



Adaptive Reuse Housing



New Housing in Paterson



New Housing in the Fourth Ward

DISTRICT STANDARDS

PATERSON FOURTH WARD



RELATIONSHIP TO THE CITY'S LAND DEVELOPMENT REGULATIONS

The Area shall be redeveloped in accordance with the standards detailed in this Redevelopment Plan. The Plan supersedes the use, bulk, and design standard provisions of the City's Land Development Regulations. Unless otherwise specified within this Plan, all definitions from the City's Land Development Ordinance shall apply.

The Board of Adjustment shall have the authority to grant relief from use standards that require a "d" variance pursuant to N.J.S.A. 40:55D-70d. The Planning Board shall have power to grant relief from other bulk and dimensional requirements of this Plan to the same extent as the Board may grant relief from bulk and dimensional requirements pursuant to N.J.S.A. 40:55D-70c.

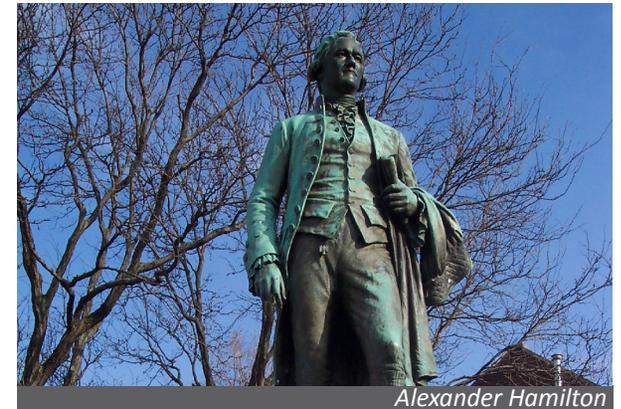
All exceptions or waivers from design standards from the requirements for site plan or subdivision approval may also be granted by the City Planning or Zoning Board.

All development must be approved by the appropriate Board and shall be submitted through the normal site plan and subdivision procedures as identified by N.J.S.A. 40:55D-1, et seq.

Final adoption of this Redevelopment Plan by the City Council shall be considered an amendment to the City Zoning Ordinance and Official Zoning Map.



Paterson City Hall



Alexander Hamilton

City of Paterson, NJ

Fourth Ward Redevelopment Plan

Proposed Districts

 Fourth Ward Redevelopment Area

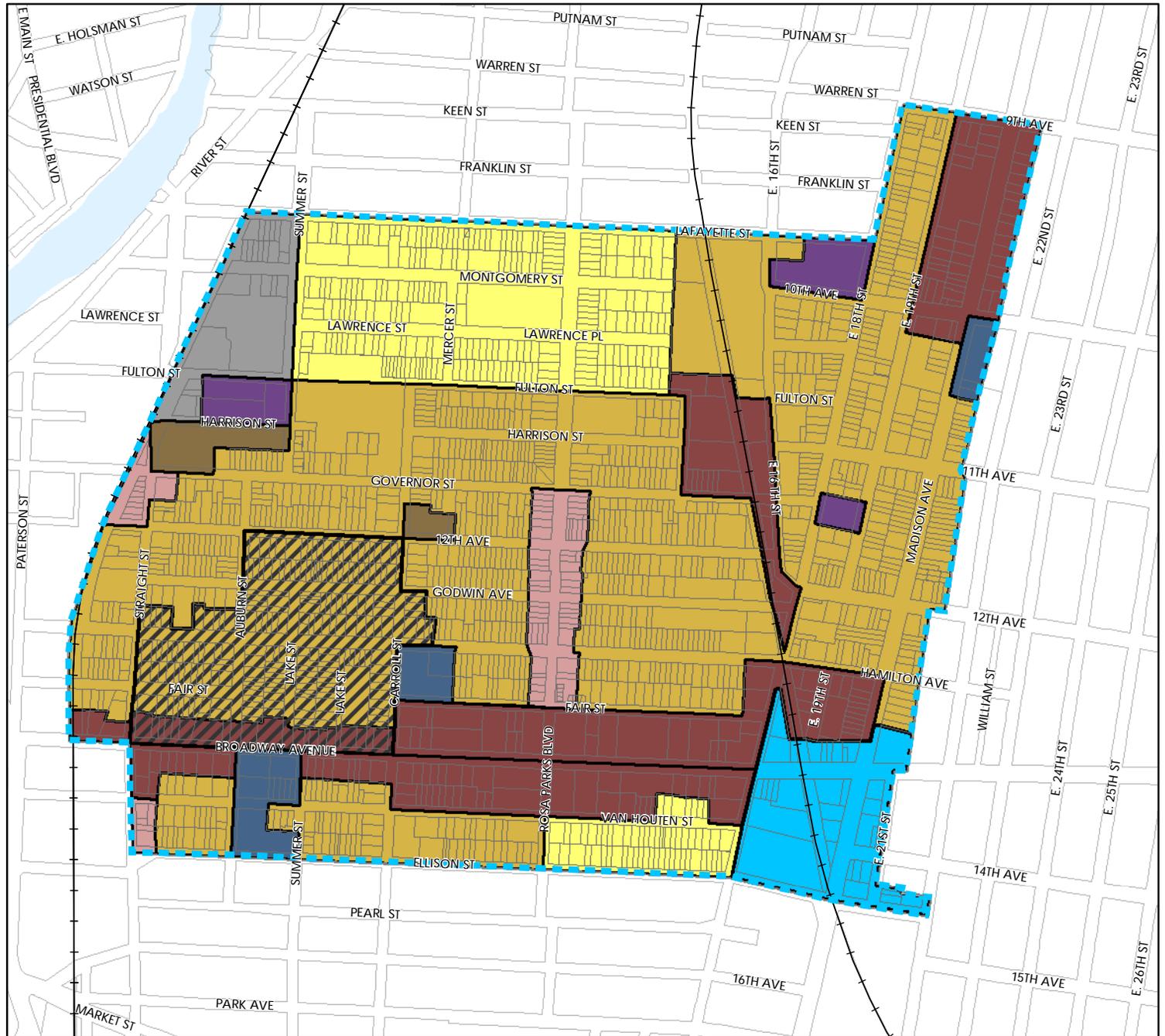
Proposed Districts

-  A-R Adaptive Reuse District
-  General Commercial
-  Light Industrial Transitional
-  Neighborhood Commercial
-  Public Use Zone
-  RA-1
-  RA-2
-  Senior Residential
-  TOD District
-  Live/Work Overlay



Source: NJGIN, NJDEP, NJOGIS, NJDOT, City of Paterson, FEMA

HEYER GRUEL & ASSOCIATES



RA-1 RESIDENTIAL DISTRICT

Intent

The intent of the RA-1 residential district is to permit single and two-family dwellings. It is the further intent of this district to maintain the residential character of the neighborhood. In keeping with this intent, multiple-family dwellings and stand-alone commercial uses are prohibited.

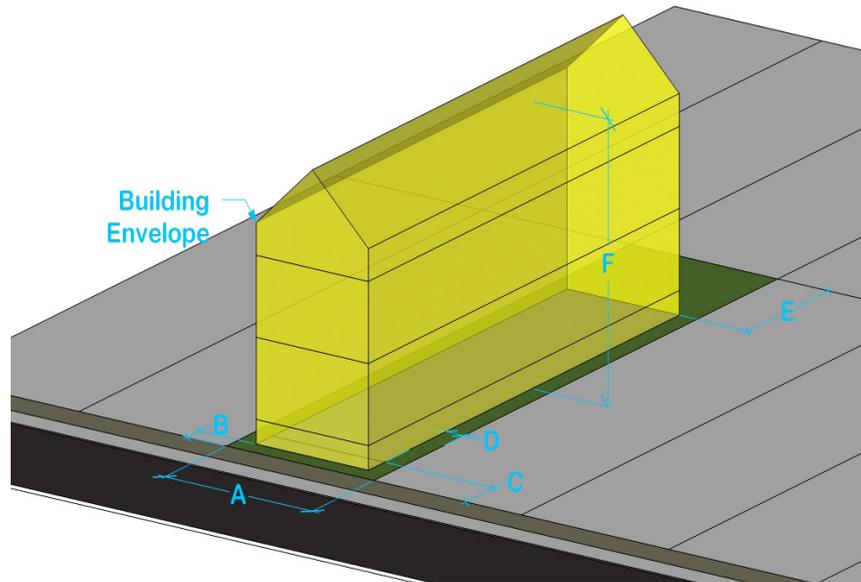
Principal Permitted Uses

- Detached Single-Family dwellings.
- Two-Family dwellings.
- Community Gardens.
- Public Facilities.
- Mixed Use Development which includes residential and neighborhood commercial uses under the following conditions:
 - Maximum lot width: fifty (50) feet.
 - These uses shall only be located at the corners of blocks. No mid-block mixed use development is permitted.
 - Neighborhood commercial uses shall only be permitted on the first floor of any building. At least one floor of residence(s) is required above the commercial use.
 - No off-street parking shall be required for the commercial use.

Accessory Uses

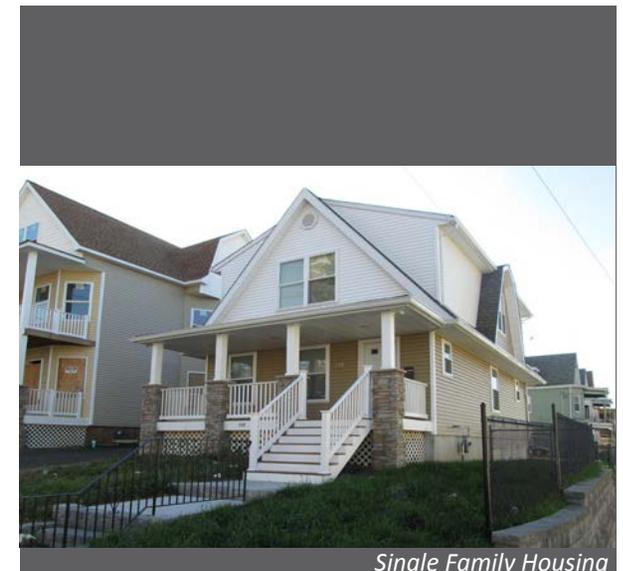
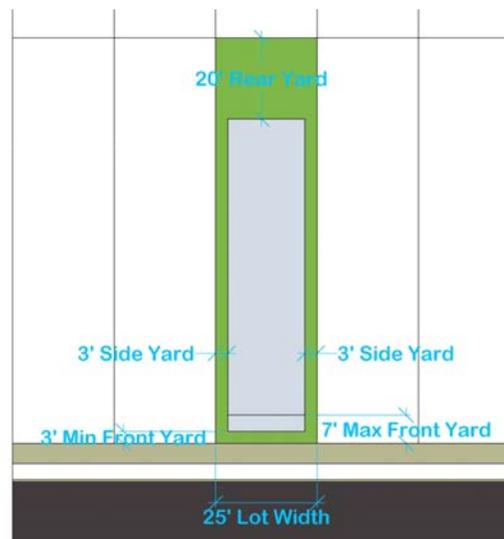
- Private garages.
- Home occupations.
- Any use customarily incidental and accessory to the principal use.

RA-1 Single/Two Family Bulk Standards Diagram



Single/Two Family Dwelling	
A	Minimum Lot Width
B	Minimum Front Yard Setback
C	Maximum Front Yard Setback
D	Minimum Side Yard Setback
E	Minimum Rear Yard Setback
F	Maximum Building Height

RA-1 Single/Two Family Bulk Standards Diagram



Parking

Lots with a width of fifty (50) feet or greater prior to subdivision and/or development shall provide off-street parking. No off-street parking is required on existing lots that are less than fifty (50) feet in width.

- Off-street parking shall be accessed by a driveway not wider than twelve (12) feet.
- The following off-street parking shall be required.
 - One (1) space per unit for dwellings with one (1) bedroom.
 - One and one-half (1.5) spaces per unit for dwellings with two (2) bedrooms.
 - An additional half (0.5) spaces per unit for each additional bedroom.
 - One (1) space per unit for residences in a mixed use development.

Usable Open Space or Amenity Space

All multiple family dwellings in the RA-2 district shall provide a minimum of 150 square feet of exterior open space or interior amenity space per dwelling unit. These spaces may include, but are not limited to landscaped outdoor areas with seating, spaces for gardening, rooftop terraces, common balconies, fitness centers, and meeting rooms.

Area, Yard and Building Requirements

See RA-1 Residential District’s Bulk Standards Chart

RA-1 District Bulk Standards	
Minimum Lot Area	2,500 square feet ¹
Minimum Lot Width	Twenty-five (25) feet ¹
Minimum Front Yard	Three (3) feet ²
Maximum Front Yard	Seven (7) feet
Minimum Side Yard	Three (3) feet
Minimum Rear Yard	Twenty (20) feet
Maximum Building Height	Forty (40) feet and three (3) stories
Maximum Building Coverage	Sixty percent (60%)
Maximum Impervious Surface Coverage	Eighty percent (80%)
<p>1. This applies only to newly created lots. Lots existing at the time of adoption of this Plan are exempt from minimum lot area and width standards and may be grandfathered in.</p> <p>2. Or the average setback within 200 feet on each side of the lot, not to exceed Seven (7) feet.</p>	

RA-2 RESIDENTIAL DISTRICT

Intent

The RA-2 residential district is designed to permit a more intensive residential use of land with various types of dwellings. Density is maintained in medium range, while building height is kept low enough to be generally compatible with one and two-family residential development.

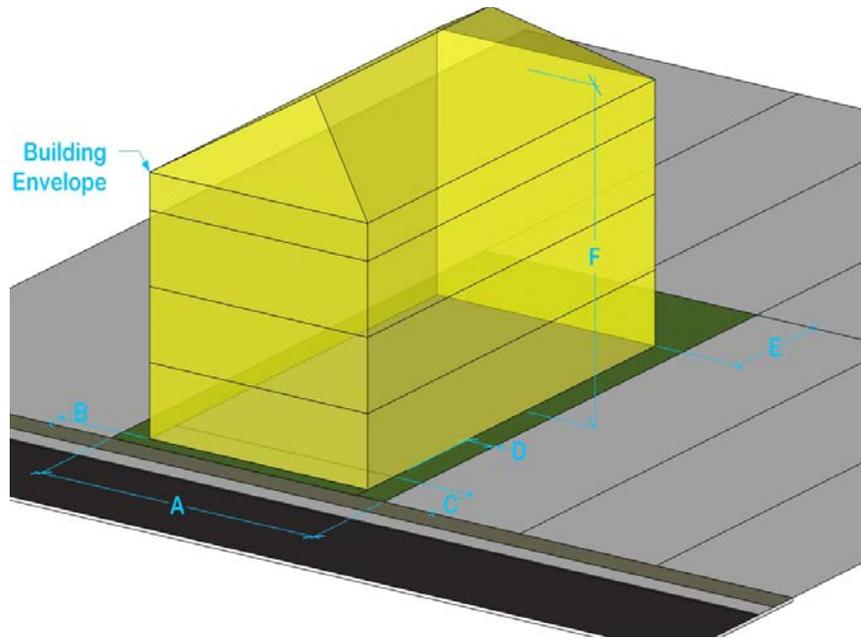
Principal Permitted Uses

- Detached Single-family dwellings.
- Attached Single-family dwellings (townhouse)
 - Maximum of four (4) units per building.
- Two-family dwellings.
- Three to Four-family dwellings.
- Community Gardens.
- Public Facilities
- Mixed Use Development which includes residential and neighborhood commercial uses under the following conditions:
 - Maximum lot width: fifty (50) feet.
 - These uses shall only be located at the corners of blocks. No mid-block mixed use development is permitted.
 - Neighborhood commercial uses shall only be permitted on the first floor of any building. At least one floor of residence(s) is required above the commercial use.
 - No off-street parking shall be required for the commercial use.

Accessory Uses

- Private garages

RA-2 Multi Family Bulk Standards Diagram

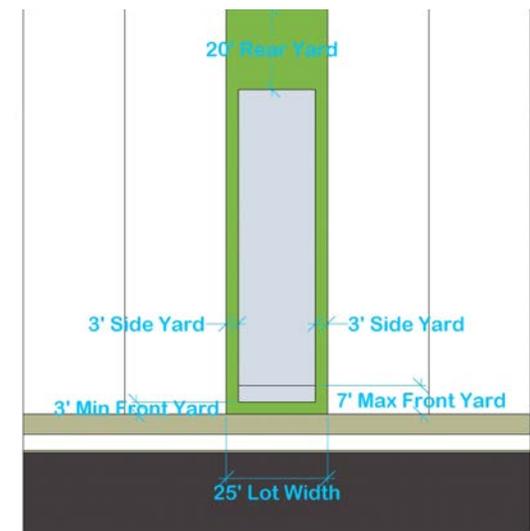


Three/Four Family Dwelling	
A	Minimum Lot Width
B	Minimum Front Yard Setback
C	Maximum Front Yard Setback
D	Minimum Side Yard Setback
E	Minimum Rear Yard Setback
F	Maximum Building Height

RA-2 Multi Family Bulk Standards Diagram



RA-2 Single/Two Family Bulk Standards Diagram



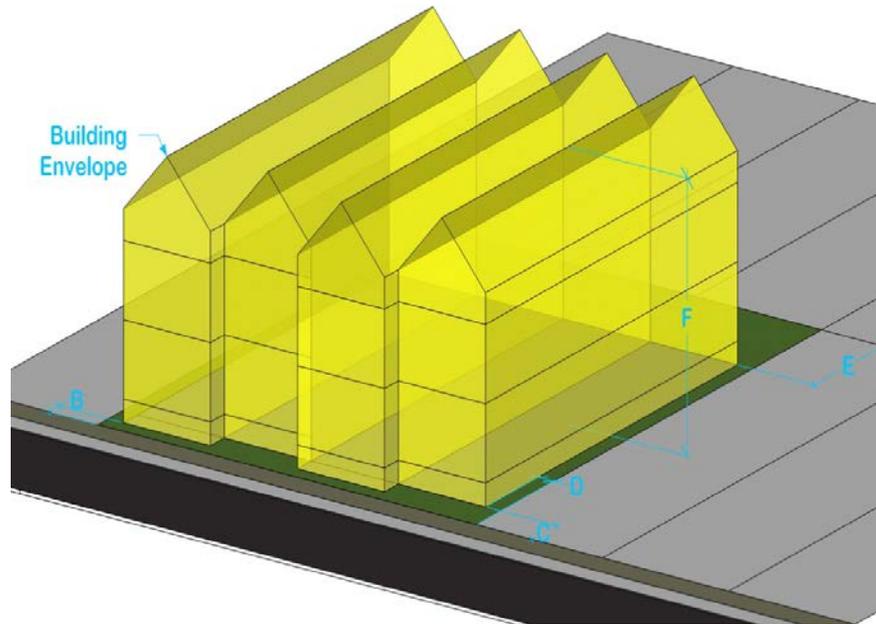
- Private greenhouses and any agricultural or horticultural activities not for business purposes, incidental to residential use; provided that any storage of fertilizer shall be distant at least fifty (50) feet from a property line
- Home occupations.
- Any use customarily incidental or accessory to the principal use.

Parking

Lots with a width of fifty (50) feet or greater prior to subdivision and/or development shall provide off-street parking. No off-street parking is required on existing lots that are less than fifty (50) feet in width.

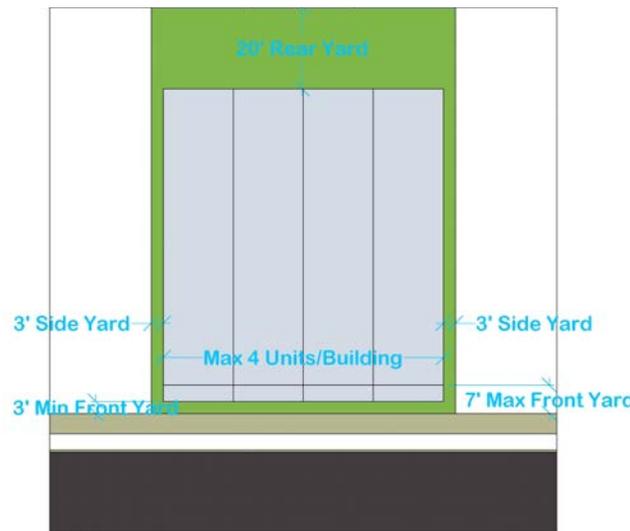
- Off-street parking shall be accessed by a driveway not wider than twelve (12) feet.
- The following off-street parking shall be required.
 - One (1) space per unit for dwellings with one (1) bedroom.
 - One and one-half (1.5) spaces per unit for dwellings with two (2) bedrooms.
 - An additional half (0.5) spaces per unit for each additional bedroom.
 - One (1) space per unit for residences in a mixed use development.

RA-2 Townhouse Bulk Standards Diagram



Townhouse Dwellings	
A	Minimum Lot Width
B	Minimum Front Yard Setback
C	Maximum Front Yard Setback
D	Minimum Side Yard Setback
E	Minimum Rear Yard Setback
F	Maximum Building Height

RA-2 Townhouse Bulk Standards Diagram



Usable Open Space or Amenity Space

All multiple family dwellings in the RA-2 district shall provide a minimum of 150 square feet of exterior open space or interior amenity space per dwelling unit. These spaces may include, but are not limited to landscaped outdoor areas with seating, spaces for gardening, rooftop terraces, common balconies, fitness centers, and meeting rooms.

Area, Yard and Building Requirements

See RA-2 Residential District’s Bulk Standards Chart

RA-2 District Bulk Standards	
Single Family or Two-Family Dwellings	
Minimum Lot Area	2,500 square feet ¹
Minimum Lot Width	Twenty-five (25) feet ¹
Minimum Front Yard	Three (3) feet ²
Maximum Front Yard	Seven (7) feet
Minimum Side Yard ³	Three (3) feet
Minimum Rear Yard	Twenty (20) feet
Maximum Building Height	Forty (40) feet and three (3) stories
Maximum Building Coverage	Sixty percent (60%)
Maximum Impervious Surface Coverage	Eighty percent (80%)
<p>1. This applies only to newly created lots. Lots existing at the time of adoption of this Plan are exempt from minimum lot area and width standards and may be grandfathered in.</p> <p>2. Or the average setback within 200 feet on each side of the lot, not to exceed Seven (7) feet.</p> <p>3. For attached dwellings (townhouses) side yard setbacks shall apply to the end of the building, and not individual units.</p>	

RA-2 District Bulk Standards	
Three-Family or Four-Family dwellings	
Minimum Lot Area	5,000 square feet
Minimum Lot Width	Fifty (50) feet
Minimum Front Yard	Three (3) feet ¹
Maximum Front Yard	Ten (10) feet
Minimum Side Yard	Five (5) feet
Minimum Rear Yard	Twenty (20) feet
Maximum Building Height	Forty-five (45) feet and Three and a half (3.5) stories
Maximum Building Coverage	Sixty percent (60%)
Maximum Impervious Surface Coverage	Eighty percent (80%)
<p>1. Or the average setback within 200 feet on each side of the lot, not to exceed Seven (7) feet.</p>	

SR SENIOR RESIDENTIAL DISTRICT

Intent

The intent and purpose of this district is to provide for high-density mid-rise senior citizen apartment residential areas.

Principal Permitted Uses

- Senior citizen housing.

Accessory Uses

- Private swimming pools and other similar recreational facilities.
- Fences and walls.
- Any use customarily incidental and accessory to the principal use.

Parking

Off-street parking shall be provided for all development in the SR District.

- Off-street parking shall be accessed by a driveway not wider than twelve (12) feet.
- The following off-street parking shall be required.
 - One (1) space per unit for dwellings with one (1) bedroom.
 - One and one-half (1.5) spaces per unit for dwellings with two (2) bedrooms.
 - An additional half (0.5) space per unit for each additional bedroom.

Area, Yard and Building Requirements

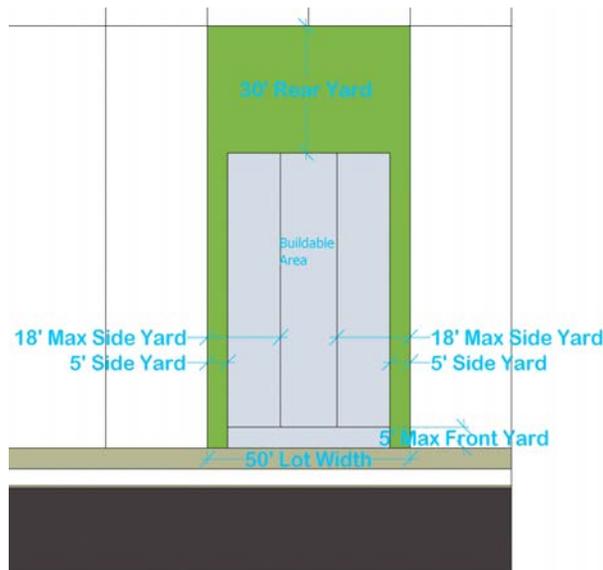
See SR Senior Residential District’s Bulk Standards Chart

Usable Open Space or Amenity Space

All multiple family dwellings in the SR district shall provide a minimum of 150 square feet of exterior open space or interior amenity space per dwelling unit. These spaces may include, but are not limited to landscaped outdoor areas with seating, spaces for gardening, rooftop terraces, common balconies, fitness centers, and meeting rooms.

S-R Senior Residential District Bulk Standards	
Minimum Lot Area	5,000 square feet
Minimum Lot Width	Fifty (50) feet
Minimum Front Yard	None
Maximum Front Yard	Five (5) feet
Minimum Side Yard (each)	Five (5) feet
Maximum Side Yard (either)	Eighteen (18) feet
Minimum Rear Yard	Thirty (30) feet
Maximum Building Height	Eighty-five (85) feet and seven (7) stories
Maximum Building Coverage	Eighty-five percent (85%)
Maximum Impervious Surface Coverage	None

SR Senior Residential Bulk Standards Diagram



AR ADAPTIVE RE-USE DISTRICT

Intent

The Adaptive Re-Use District is intended to provide for the re-use of existing historic buildings by adapting them to house residential units and compatible commercial uses. The intent of this district is to maintain the existing historic buildings to the greatest extent feasible, while allowing them to be utilized to meet community needs.

Principal Permitted Uses

- Multi-family residences.
- Retail stores and shops.
- Personal service businesses.
- Financial Institutions.
- Business or professional offices.
- Hotels.
- Restaurants (excluding drive-in or drive-thru restaurants).
- Public Facilities.
- Mixed Use Development of any combination of the principal permitted uses.

Accessory Uses

- Off-street parking
- Any uses customarily incidental and accessory to the principal permitted uses(s) of the property

Parking

- The following off-street parking shall be required.

- Retail stores and personal service businesses: One (1) space per one-thousand (1,000) square feet.
 - Business and professional offices: One (1) space per one-thousand (1,000) square feet.
 - Financial institutions: One (1) space per one-thousand (1,000) square feet.
 - Restaurants: One (1) space per eight (8) seats.
 - Residences: One (1) space per residential unit.
 - Hotels: One (1) space per guest room.
- Off-street parking may be provided off-site on another parcel that is located no greater than three-hundred (300) feet from the property.

Area, Yard, and Building Requirements

See AR Adaptive Re-Use District’s Bulk Standards Chart

AR Adaptive Re-Use District Bulk Standards	
Minimum Lot Area	None
Minimum Lot Width	None
Minimum Front Yard ¹	Existing building footprint shall be maintained
Minimum Side Yard ¹	Existing building footprint shall be maintained
Minimum Rear Yard ¹	Existing building footprint shall be maintained
Maximum Building Height ²	New construction may extend a maximum of two (2) stories or twenty-five (25) feet above the existing building roofline.
Maximum Building Coverage	Existing building footprint
Maximum Impervious Surface Coverage	None
1. New Construction or additions to the existing buildings may extend beyond the existing building footprint by no more than four (4) feet. 2. New construction above the existing roofline must be set back a minimum of ten (10) feet from the existing roofline on all sides. 3. Building Coverage may be permitted to expand no greater than five (5%) of the total lot, beyond existing building coverage.	

C-1 NEIGHBORHOOD COMMERCIAL DISTRICT

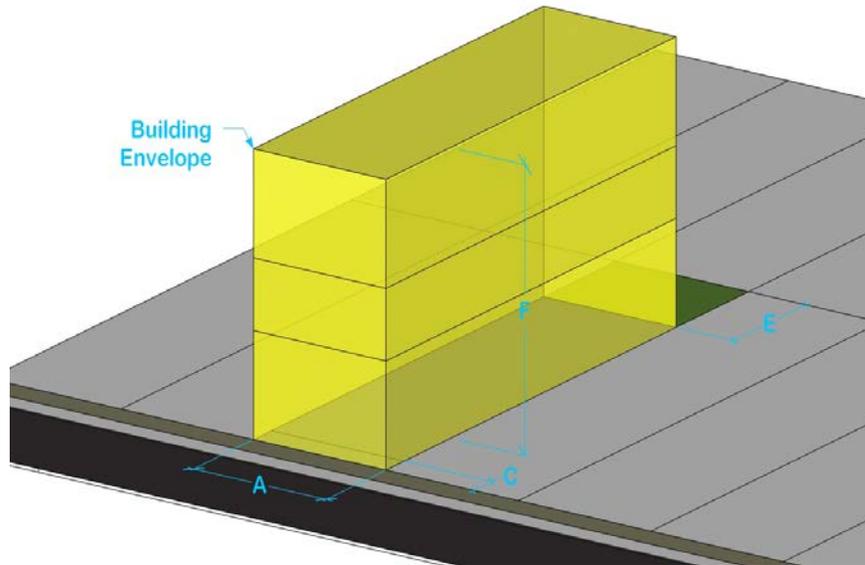
Intent

The district is designed to provide for retail and personal service businesses which supply the everyday needs of the neighborhood residents. The intent of the district standards are to allow for convenience retail and service businesses to operate without disrupting the general small-scale residential character of the greater neighborhood area.

Principal Permitted Uses

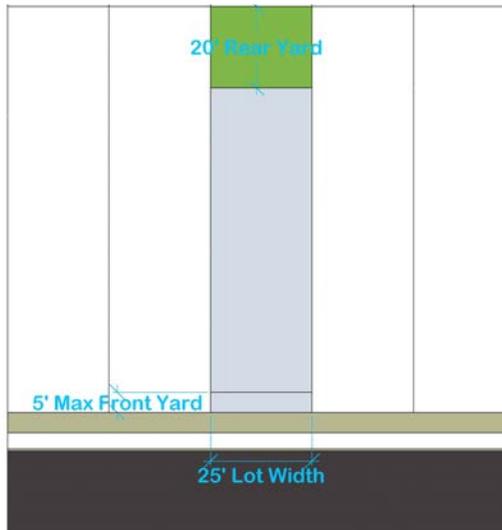
- Retail stores and shops.
- Personal service businesses
- Restaurants (excluding drive-in or drive-thru restaurants)
- Banks and financial institutions (excluding drive thru service)
- Dry cleaning and clothing laundries occupying not more than 3,000 square feet of floor area; provided further that no central plant servicing more than one (1) retail outlet shall be provided. Any establishment of this nature which occupies more than 3,000 sq. ft. shall be considered an industrial establishment and shall be prohibited.
- Child Care Centers.
- Public Facilities.
- Mixed Use Development which includes residential uses and permitted neighborhood commercial uses:
 - Residential uses shall only be permitted above the ground floor of a building.
- Business and Professional Offices.
 - These uses shall only be permitted

C-1 Neighborhood Commercial Bulk Standards Diagram



Neighborhood Commercial	
A	Minimum Lot Width
B	Minimum Front Yard Setback
C	Maximum Front Yard Setback
D	Minimum Side Yard Setback
E	Minimum Rear Yard Setback
F	Maximum Building Height

C-1 Neighborhood Commercial Bulk Standards Diagram



Neighborhood Commercial Retail

- above the ground floor of a building.
- Travel Agencies and Real Estate offices.

Area, Yard and Building Requirements

See C-1 Neighborhood Commercial District’s Bulk Standards Chart

Accessory Uses

- Any uses customarily incidental and accessory to the principal use of the property.

Parking

Lots with a width of fifty (50) feet or greater prior to subdivision and/or development shall provide off-street parking. No off-street parking is required on existing lots that are less than fifty (50) feet in width.

- The following off-street parking shall be required.
 - Retail stores, personal service businesses, and dry cleaning: One (1) space per six-hundred (600) square feet.
 - Financial institutions: One (1) space per six-hundred (600) square feet.
 - Restaurants: One (1) space per four (4) seats.
 - Business and professional offices: One (1) space per one-thousand (1,000) square feet.
 - Travel agencies and real estate offices: One (1) space per one-thousand (1,000) square feet.
 - Child care centers: One (1) space per employee on maximum shift.
 - Residential apartments in mixed use buildings: One (1) space per dwelling unit.

C-1 Neighborhood Commercial District Bulk Standards	
Minimum Lot Area	2,000 square feet
Minimum Lot Width	Twenty-five (25) feet
Minimum Front Yard	None
Maximum Front Yard	Five (5) feet
Minimum Side Yard (each)	None
Minimum Rear Yard	Twenty (20) feet
Maximum Building Height	Forty (40) feet and three (3) stories
Maximum Building Coverage	Eighty percent (80%)
Maximum Impervious Surface Coverage	One-hundred percent (100%)



Neighborhood Commercial Retail

C-2 GENERAL COMMERCIAL DISTRICT

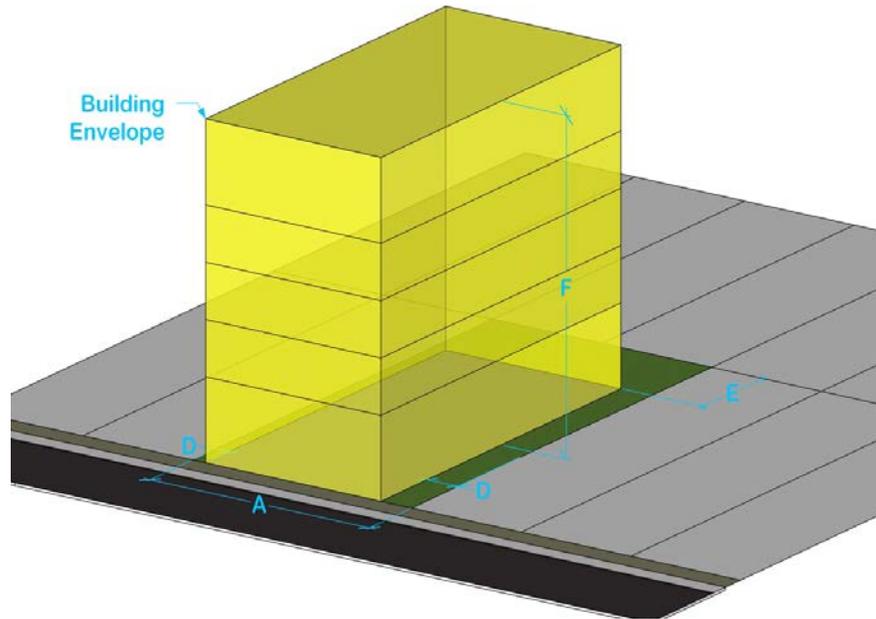
Intent

The intent of the C-2 general commercial district is to provide sufficient space in appropriate locations for a wide variety of community commercial and service activities, serving a wider area than the C-1 district. This district is located near the intersections of or along major thoroughfares where a general mixture of commercial and service activity now exists, but which uses are not characterized by extensive warehousing or frequent heavy truck activity.

Principal Permitted Uses

- Retail stores and shops.
- Personal service businesses.
- Business and Professional offices.
- Art galleries.
- Restaurants (including drive-in or drive-thru restaurants)
 - Drive-thru restaurants must provide a designated drive-thru lane that does not obstruct pedestrian or vehicular traffic, with sufficient capacity for stacking of a minimum of five (5) vehicles.
- Banks and financial institutions
 - Drive-thru facilities are permitted provided that a dedicated drive-thru lane which does not obstruct pedestrian or vehicular traffic, with sufficient capacity for a minimum of five (5) vehicles, is provided.
- Dry cleaning and clothing laundries occupying not more than 3,000 square feet of floor

C-2 General Commercial Bulk Standards Diagram



General Commercial	
A	Minimum Lot Width
B	Minimum Front Yard Setback
C	Maximum Front Yard Setback
D	Minimum Side Yard Setback
E	Minimum Rear Yard Setback
F	Maximum Building Height

C-2 General Commercial Bulk Standards Diagram



area; provided further that no central plant servicing more than one (1) retail outlet shall be provided. Any establishment of this nature which occupies more than 3,000 sq. ft. shall be considered an industrial establishment and shall be prohibited.

- Funeral homes.
- Child Care Centers.
- Public Facilities.

- One (1) space per one-thousand (1,000) square feet.
- Funeral Homes: One (1) space per one-hundred (100) square feet of floor area in viewing rooms.
- Child care centers: One (1) space per employee on maximum shift.

Area, Yard and Building Requirements

See C-2 General Commercial District’s Bulk Standards Chart

Accessory Uses

- Any uses customarily incidental and accessory to the principal use of the property.

Parking

Off-street parking is required for all development in the C-2 District.

- The following off-street parking shall be required.
 - Retail stores, personal service businesses, and dry cleaning: One (1) space per six-hundred (600) square feet.
 - Financial institutions: One (1) space per six-hundred (600) square feet.
 - Restaurants: One (1) space per four (4) seats.
 - Galleries: One (1) space per five-hundred (500) square feet of floor area.
 - Business and professional offices: One (1) space per one-thousand (1,000) square feet.
 - Travel agencies and real estate offices:

C-2 General Commercial District Bulk Standards	
Minimum Lot Area	5,000 square feet
Minimum Lot Width	Fifty (50) feet
Minimum Front Yard	None
Minimum Side Yard (each)	Three (3) feet
Minimum Side Yard (both)	Ten (10) feet
Minimum Rear Yard	Twenty (20) feet
Maximum Building Height	Sixty (60) feet and five (5) stories
Maximum Building Coverage	Eighty percent (80%)
Maximum Impervious Surface Coverage	One-hundred percent (100%)



LIT LIGHT INDUSTRIAL TRANSITION DISTRICT

Intent

The intent of this district is to provide sufficient space in appropriate locations for a wide variety of light industrial and warehousing activities. This district is located in former heavy manufacturing areas, which are undergoing transformation to residential neighborhoods, but which are adjacent to active industrial uses. This district is intended to provide a transition between industrial activities and residential neighborhoods. As such, light industrial and warehousing activities are required to provide a high degree of site amenities, such as landscaping and fencing to provide a buffer from surrounding residences.

Principal Permitted Uses

- Light Industrial Uses
- Research and Development Laboratories.
- Business or Professional Offices.
- Warehousing and Distribution Facilities.
- Self-Storage Facilities.
- Public Uses.

Accessory Uses

- Any uses customarily incidental and accessory to the principal permitted use of the property.

Parking

Off-street parking is required on for all development in the LIT District.

- The following off-street parking shall be required.
 - Light Industrial Uses: One (1) space per one-thousand (1,000) square feet of gross floor area.
 - Research and Development Laboratories: One (1) space per one-thousand (1,000) square feet of gross floor area.
 - Warehousing and Distribution: One (1) space per five-thousand (5,000) square feet of gross floor area.
 - Business and professional offices: One (1) space per one-thousand (1,000) square feet of net leasable floor area.
 - Self-Storage Facilities: One (1) space per fifty (50) storage units.
 - Loft Dwellings or Live/Work Quarters: One (1) space per dwelling unit.

Area, Yard and Building Requirements

See LIT Light Industrial Transition District’s Bulk Standards Chart

Additional Requirements

- Storage and Loading areas for any use shall not be located between the building line and the street, and must be screened with a combination of fencing and landscaping.
- Where a lot is adjacent to a residential use or residentially zoned property, a landscaped buffer with a minimum width of ten (10) feet must be provided.
 - Buffers shall be designed to be integral to any site plan and building arrangement.

LIT Light Industrial Transition District Bulk Standards	
Minimum Lot Area	40,000 square feet
Minimum Lot Width	Fifty (50) feet
Minimum Front Yard	Thirty-five (35) feet
Minimum Side Yard (each)	Thirty (30) feet
Minimum Rear Yard	Thirty (30) feet
Maximum Building Height	Sixty (60) feet and five (5) stories
Maximum Building Coverage	Fifty percent (50%)
Maximum Impervious Surface Coverage	Eighty-five percent (85%)

- No off-street parking or storage may be permitted within any required buffer area.
- Buffers shall be planted with dense evergreen trees or shrubs with a minimum height of five (5) feet.



Light Industrial Transition District

PU PUBLIC USE DISTRICT

Intent

The intent of this district is to create areas for public and quasi-public uses located close to commercial centers, but still accessible to much of the neighborhood.

Principal Permitted Uses

- Governmental Facilities.
- Schools.
- Community Centers.
- Recreation and Open Space

Accessory Uses

- Any uses customarily incidental and accessory to the principal permitted use of the property

Parking

Off-street parking is required for all development in the Public Use District.

- The following off-street parking shall be required.
 - Schools: One (1) space per each classroom, plus four (4) spaces for administration.
 - Other Uses: Appropriate off-street parking for other uses shall be at the discretion of the Planning Board, based on accepted standards and needs of the particular use.



Public School Number 21



Public School Number 6



Public School Number 6

TRANSIT ORIENTED DEVELOPMENT DISTRICT

Intent

The intent of this district is to permit higher density mixed use, commercial and residential development around a potential light rail stop in the Area, in conformity with the recommendations of the Madison Avenue Commuter Rail Corridor Study.

Principal Permitted Uses

- Multi-family residences.
- Retail stores and shops.
- Personal service businesses.
- Financial Institutions.
- Business or professional offices.
- Hotels.
- Restaurants (excluding drive-in or drive-thru restaurants).
- Public Facilities.
- Mixed Use Development of any combination of the principal permitted uses.
- Public Structured Parking

Accessory Uses

- Off-street parking
- Structured parking garages
- Any uses customarily incidental or accessory to the principal use(s) of the property

Usable Open Space or Amenity Space

All multiple family dwellings in the TOD district shall provide a minimum of 150 square feet of exterior open space or interior amenity space per dwelling unit. These spaces may include, but

are not limited to landscaped outdoor areas with seating, spaces for gardening, rooftop terraces, common balconies, fitness centers, and meeting rooms.

Parking

- The following off-street parking shall be required.
 - Stand-alone Retail stores and personal service businesses: One (1) space per one-thousand (1,000) square feet.
 - Business and professional offices: One (1) space per one-thousand (1,000) square feet.
 - Financial institutions: One (1) space per one-thousand (1,000) square feet
 - Restaurants: One (1) space per eight (8) seats.
 - Residences: One (1) space per residential unit.
 - Hotels: One (1) space per guest room.
 - The retail component of a mixed-use residential-retail development does not require off-street parking.
- Off-street parking may be provided off-site on another parcel that is located no greater than three-hundred (300) feet from the property.
- No parking may be permitted in the front yard area.
- Structured parking garages may have frontage on a public street, and shall be located interior to any parcel.

Area, Yard and Building Requirements

See TOD Transit Oriented Development District Bulk Standards Chart

TOD Transit Oriented Development District Bulk Standards	
Minimum Lot Area	None
Minimum Lot Width	None
Minimum Front Yard	None
Maximum Front Yard	Five (5) feet
Minimum Side Yard	None
Minimum Rear Yard	Twenty (20) feet
Maximum Building Height	Seventy (70) feet and 6 stories
Maximum Building Coverage	100%
Maximum Impervious Surface Coverage	100%



Mixed Use Transit Oriented Development

LIVE/WORK COMMUNITY OVERLAY DISTRICT

Intent

The intent of this district is to provide for the opportunity for development of a mixed-use, live/work community of creative entrepreneurs. This community would be constructed of small residential apartment units that are connected to small retail shops or production spaces in a live/work accomodation where the residents of the apartments work in the shops below their apartment.

Principal Permitted Uses

- Multi-family residences.
- Retail stores and shops.
- Personal service businesses.
- Professional offices.
- Medical and dental offices.
- Grocery stores.
- Restaurants.
- Bakeries and cafes.
- Micro-manufacturing and production spaces for goods to be sold or distributed on the premises.
- Artist’s studios.
- Any other use similar in character and intensity to these principal permitted uses.
- Mixed Use Development of any combination of the principal permitted uses.

Accessory Uses

- Any uses customarily incidental or accessory to the principal uses.

Parking

- The following off-street parking shall be required:
 - Residences: One (1) space per four (4) residential units.
 - No off-street parking is required for any non-residential use of the property.
- Off-street parking may be provided off-site on another parcel that is located no greater than three-hundred (300) feet from the property.
- No parking may be permitted in the front yard area.

Area, Yard and Building Requirements

See Live/Work Community Overlay District’s Bulk Standards Chart

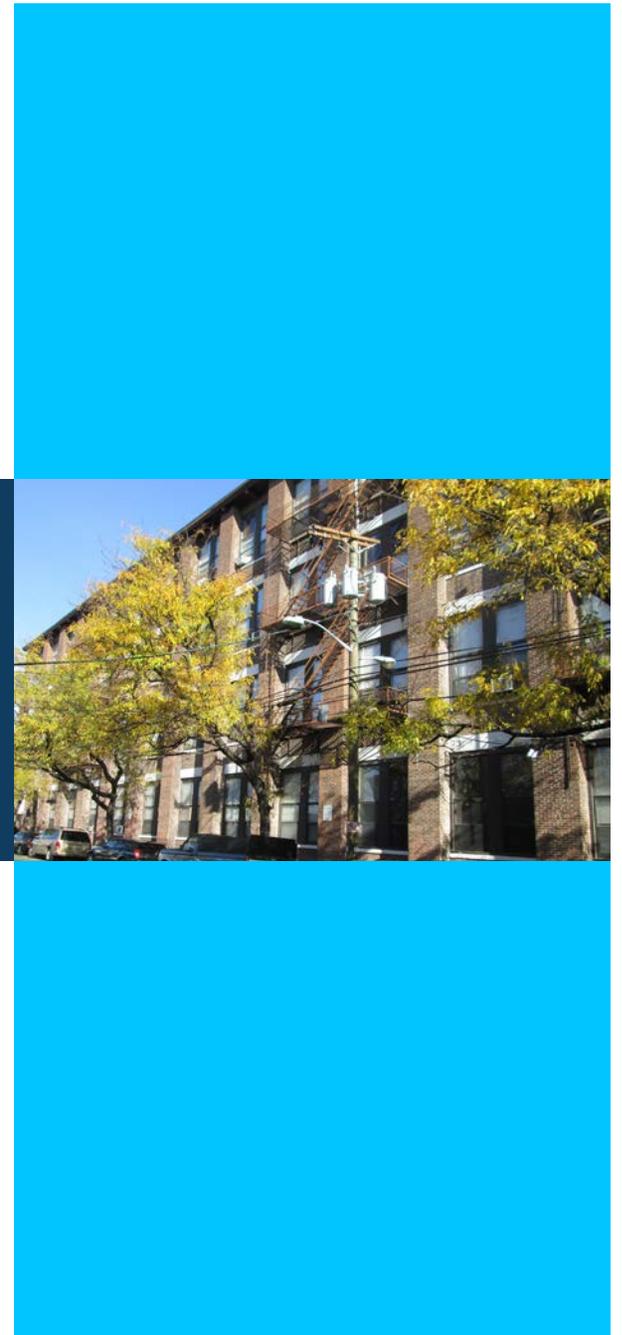
Other Requirements

- The provisions of this overlay district shall only be applicable under the condition that live/work units are constructed, with a deed restriction that stipulates that the owner or operator of any non-residential use must reside in an apartment unit on the premises.
- A minimum of four (4) work/shop units must constructed on the site.
- Retail shops, restaurants, bakeries, and cafes must be located on the ground level.
- The ratio of apartments to work/shop spaces shall not exceed 4:1.

Live/Work Community Overlay District Bulk Standards	
Minimum Lot Area	5,000 square feet
Minimum Lot Width	None
Minimum Front Yard	None
Maximum Front Yard	None
Minimum Side Yard (each)	None
Minimum Rear Yard	None
Maximum Building Height	None
Maximum Floor Area Ratio (FAR)	6.0
Maximum Building Coverage	None
Maximum Impervious Surface Coverage	None

DESIGN STANDARDS

PATERSON FOURTH WARD



GENERAL DESIGN STANDARDS

The purpose of the design standards for the Fourth Ward Redevelopment Plan is to ensure that the quality of construction, and general design and aesthetic of all new construction meets certain minimum standards. These standards are meant to encourage design that is sensitive to the urban context of the Fourth Ward of the City of Paterson, and does not in any way detract or denigrate the character of the existing community.

New construction or proposed renovations must maintain continuity in design with the larger physical context of the Area. Excessive contrast may overwhelm the surrounding existing structures. New construction or renovations should take cues from the surrounding buildings. Continuity in streetscape elements such as sidewalks, street trees, light fixtures, signs, and plantings is also desired.

General Residential Design Standards

Paterson’s housing stock is eclectic, featuring an array of housing types and architectural styles. While no one style predominates, the best housing in Paterson is of a traditional urban pattern characterized by front porches, small setbacks, vertical windows, sloping roofs, traditional materials, coordinated color schemes, and architectural features such as gables and dormers. These properties also contain amenities such as street trees and ornamental fencing. All new housing should emulate and conform to this traditional urban pattern.

While houses should be based on the traditional urban pattern, there should be some variety in

the forms that these patterns take. As such, there should be a variety of roof and porch styles, window groupings, façade colors, and façade details such as shutters and sash trim. New construction or renovations should strive to strike a balance between consistency and variety. Housing should be oriented to the existing street network and should generally follow traditional urban design principles.

Encouraged

- Hedges to help delineate public/private space
- High quality ornamental wooden/metal fencing.
- Street trees.
- Shutters.
- Sitting walls/porches/stoops.
- Attractive color schemes.
- Landscaping.
- Architectural details such as motifs, cornices, and decorative window frames.
- Porches or stoops to create a physical and psychological separation between the private space of the building and the public space of the street.

Discouraged

- Barred windows.
- Lack of porches/stoops.
- Lack of delineation between public/private space. (fencing, hedges, etc.)
- Chain link fences.
- Front yard parking.
- No street trees, minimal landscaping.





Appropriate Mixed Use/Commercial Buildings



Poor Commercial Storefront



Poor Commercial Storefront

General Commercial Design Standards

These standards are intended to enhance and reinforce the physical and visual characteristics of the commercial districts in the Fourth Ward Redevelopment Area. They are to be used in conjunction with the district zoning and bulk standards that are established in this Redevelopment Plan. The general façade design standards are to be applied to renovations of existing structures, infill construction and new construction on vacant property.

The intent of these standards is to encourage a lively and attractive retail shopping atmosphere in front of commercial buildings.

Encouraged

- Visual coordination of building elements.
- Stores built at front lot lines framing pedestrian activity on sidewalk.
- Open and transparent retail storefronts.
- Cornice and window lines coordinated with adjacent buildings.
- Awning lines matching in old and new construction.
- Security gates designed to blend into the streetscape.
- Security gates installed on the inside of entrance doors rather than the outside.

Discouraged

- Excessive signage.
- Windows and storefronts covered with signage or blocked by merchandise.
- Blank or featureless walls.
- Obtrusive security grates

RESIDENTIAL ARCHITECTURAL DESIGN STANDARDS

This section governs the architectural design of all residential structures in the Fourth Ward Redevelopment Area.

Permitted Exterior Building Materials

- Brick
- Natural stone
- Wood (pressure treated or naturally decay resistant)
- Stucco
- EIFS (above the first floor only)

Windows and Doors

- Openings for windows and window panes shall have a vertical dimension that is greater than the horizontal dimension.
- If exterior shutters are used, they shall be sized appropriately to match the window opening.
- Windows may be grouped to create a horizontal composition, with a maximum of three windows grouped together.
- Window sills shall project a minimum of 2 inches from the building face.
- Windows shall not be placed flush with the exterior building wall, but shall be centered within the wall cavity.
- Where siding is used, all openings for windows or doors shall be cased. Trim boards must have a minimum nominal dimension of 1"x4" on sides and sill, and a nominal dimension of 1"x6" for the head.
- All header trim around openings shall be at

least 2 inches taller than the width of the adjacent trim.

Residential Roofs

- All Single and Two Family residential structures must have pitched or sloped roofs.
- All hipped and gabled roofs shall have eaves that overhang a minimum of 18 inches from the building face.
- A hipped or gabled roof shall have a minimum pitch of 8:12.
- Dormers shall light habitable spaces, and if gabled, shall have a pitch to match that of the principal roof structure.
- Roof and wall penetrations, other than chimneys, shall be concealed from public view to the maximum extent feasible.
- Where a building has a flat roof, a cornice shall be included at the top of the building on any front-facing facade that projects a minimum of 1 foot from the building face.

Other Design Standards

- Exterior building walls shall be consistent in architectural quality and detail on all elevations visible from the public street.
- Foundation exposures shall not be permitted in the front of a building.
- Porches and stoops shall be a minimum of 2 feet above the adjacent sidewalk elevation.
- If covered, stoops and porches shall be covered by a high quality roofing material, such as metal, slate, concrete, or clay tile.
- Porches and stoops shall not be screened or otherwise enclosed.
- The foundation walls of porches and stoops

shall be finished to match the exterior of the building.

- Projecting elements on the building face such as bay windows or balconies may encroach up to 4 feet into the front yard setback beginning above the first floor.



RESIDENTIAL ARCHITECTURAL DESIGN STANDARDS



ENCOURAGED

- Consistent scale and massing.
- Front porches create transition space between public realm and house.
- Varied building materials and window configurations.
- Varied roof forms.
- Dormer windows provide light into upper half story.
- Traditional residential design.

DISCOURAGED

- Parking in front of residences.
- Lack of porches/stoops to delineate public and private spaces, and create transition space.
- Homes set back from street.
- Chain link fencing in front yard.



COMMERCIAL ARCHITECTURAL DESIGN STANDARDS

This section governs the architectural design of all commercial structures in the Fourth Ward Redevelopment Area. Where a mixed use building is proposed, the commercial design standards shall apply for the commercial portions of a building, while the residential standards shall apply for the residential portions of a building.

Permitted Exterior Building Materials

- Brick
- Natural stone
- Wood
- Stucco
- Concrete (smooth or stucco finish)
- Steel and glass
- EIFS (above the first floor only)

Retail Fenestration

- A minimum of 50% of the ground floor wall surface facing the public street shall be glazed.
- Glazing shall be a minimum of 70% transparent.
- A maximum of 20% of the glazing on storefronts and/or doors may be blocked by window signs or advertisements.
- Collapsible grill doors are prohibited from being used on the outside of a storefront or entrance.

Commercial Roofs

- Where a building has a flat roof, a cornice

shall be included at the top of the building on any front facing facade, that projects a minimum of 1 foot from the building face.

- Buildings with flat roofs shall have a parapet along the entire front facade that extends a minimum of 2 feet above the roofline.
- Where a commercial building has a sloped roof, the residential design standards for sloped roofs shall apply.
- Roof and wall penetrations, other than chimneys, shall be concealed from public view to the maximum extent feasible.
- Mechanical equipment located on a roof shall be screened from public view.

Other Design Standards

- Awnings, marquees, canopies, and similar permitted design elements may project up to 4 feet into the front yard setback at a height of at least 8 feet above sidewalk grade.
- Awnings shall have a metal frame, and be covered by canvas or a like material.
- Exterior building walls shall be consistent in architectural quality and detail on all elevations visible from the public street.
- Foundation exposures shall not be permitted in the front of a building.



Building Cornice and Parapet



Commercial Awning



Open and Clear Commercial Storefront

COMMERCIAL ARCHITECTURAL DESIGN STANDARDS



ENCOURAGED

- Consistent building scale and massing.
- Cornice and window lines coordinated.
- Varied building materials and configuration.
- Large, open windows for commercial/retail storefront.
- Mixed use of buildings with residences or office space above commercial uses.
- Storefront designed with signage area.
- Buildings front on public Right-of-Way, creating a sense of enclosure on the street.

DISCOURAGED

- Lack of open storefront.
- Excessive signage.
- Windows blocked by merchandise and signage.



STREETSCAPE STANDARDS

Residential Streetscapes

Residential streetscapes are defined as both the public and private spaces located between the edge of the curb to the front of a house or dwelling. The quality of streetscapes goes a long way towards defining the character of a neighborhood. Neighborhoods with large trees and leafy front gardens make residents feel better. Neighborhoods with quality streetscapes encourage more people on the street and are therefore inherently safer. Properties located in attractive, safe neighborhoods generally have higher value and are more desirable to live in.

Paterson has many fine residential neighborhoods and attractive streetscapes located throughout the city. These residential streetscape standards seek to emulate the basic design principles that make these neighborhoods so attractive and comfortable and apply them throughout this Redevelopment Area.

Streetscapes have both a public and private realm. The public realm extends from the outside edge of the curb line to the edge of the public right of way, which usually corresponds with the inside edge of sidewalk. This realm includes the curb, planting strip and sidewalk and usually contains public amenities such as street trees, light poles, telephone poles and mailboxes.

The private realm extends from the edge of the public right of way to the front of a house or dwelling. This realm is where the private house meets the public street. Some houses may be set

flush with the right of way and not have any private space, while others may be set back quite a bit and contain porches, driveways, fences, walls, and/or landscaping.

Residential Streetscape Standards

- Sidewalks shall be constructed on both sides of all public streets.
- Sidewalks shall be a minimum of 5 feet in width.
- Sidewalks shall be constructed of a material that is different from that of the roadway.
- A planting strip for street trees of at least 3 feet in width shall be located adjacent to the sidewalk, between the sidewalk and the curb.
- The curb between the planting strip and the street shall be a minimum of 6 inches in width, and a maximum of 8 inches in width.
- Garden walls in the front yard are permitted to be no taller than 3 feet in height.
- Front yard setback areas shall be landscaped with a combination of appropriate flowering trees, shrubs, perennials, and/or low-maintenance groundcovers.
- Front lawns and turf are discouraged where the front yard setback is less than 5 feet.
- Efforts shall be made to make utilities as unobtrusive as possible.
- Transformers, generators, and other mechanical equipment shall not be located on the sidewalk, or within the required front yard area.
- Utilities shall be located underground, unless site specific constraints prohibit this.

Commercial Streetscapes

Commercial streetscapes are defined as the area between the outside edge of the curb to the front of a building. Unlike buildings in residential areas, commercial buildings are usually set flush with the edge of the right of way, with little or no setback. Even with minimal areas for landscaping or planting, commercial streetscapes can be enhanced to create attractive, safe pedestrian environments. The parts of businesses visible from the street for commercial uses should be designed to be as attractive and inviting as possible.

Retail businesses contribute to the streetscape by how they are experienced at the pedestrian level. Businesses that are clean, open, and visually interesting positively reinforce the streetscape. Businesses that are cluttered, shuttered, and lack amenities discourage pedestrian traffic. The major features that define the streetscape in a retail environment are windows, signage, awnings, and lighting. It should be noted that retail businesses with large, open windows, decorative lighting, and simple awnings are more desirable and attract more business than retail buildings without.

Commercial Streetscape Standards

- Crosswalks shall be a minimum of 5 feet in width, and located on all sides of all intersections of roads where commercial uses are present, or permitted.
- Sidewalks shall be provided to all entrances of all buildings.
- Sidewalks shall be constructed of a material that is different from that of the roadway.
- Sidewalks shall be a minimum of 5 feet in

STREETSCAPE DESIGN STANDARDS



RESIDENTIAL STREETSCAPE

- Planted landscape buffer strip between street and sidewalk.
- Front porches open to street.
- Small, landscaped front yards rather than expansive lawns.
- Street trees along both sides of street, providing shade.



COMMERCIAL STREETSCAPE

- Paver strip delineates sidewalk area from street.
- Street trees planted within paver strip, covered with tree grates.
- Bicycle parking provided.
- Buildings and storefronts line the street.
- Large open storefront windows.
- Outdoor seating and pedestrian amenities provided.
- Street lights spaced between street trees to avoid blockage of light.

width.

- A planting/paver strip for street trees and alternate paving materials of at least 4 feet in width shall be located adjacent to the sidewalk, between the sidewalk and the street.
- The curb between the planting strip and the street shall be a minimum of 6 inches in width, and a maximum of 8 inches in width.
- Front yard setback areas shall contain a combination of hardscape (sidewalk) and landscaped areas.
- Street furniture such as benches, planters, or newspaper boxes shall be located within either the front yard area, or the required planting/paver strip. Sidewalks shall not be impeded by street furniture.
- Trash and recycling receptacles for public use shall be located near all intersections within commercial use areas.
- Outdoor storage is not permitted in the front yard area.
- All outdoor storage in side or rear yard areas must be screened from public view with a solid fence.
- Efforts shall be made to make utilities as unobtrusive as possible.
- Transformers, generators, and other mechanical equipment shall not be located on the sidewalk, or within the required front yard area.
- Utilities shall be located underground, unless site specific constraints prohibit this.
- Outdoor dining shall be permitted on the sidewalk in front of restaurants, cafes, and other eating and drinking establishments provided that a minimum effective sidewalk width of 5 feet is maintained.

- Outdoor dining shall not block or impede any vehicular or pedestrian entrances.
- Restaurants must provide for the disposal of waste and recyclable materials on their own. Public garbage and recycling receptacles on the street shall not be used by restaurants or their patrons.
- All food preparation for outdoor dining shall be conducted within the regular kitchen area of the restaurant.

Complete Streets & Green Streets Standards

In addition to the traditional streetscape standards listed above, developers should be encouraged to pursue environmentally friendly and sustainable “green” streetscape designs that encourage pedestrian activity, help to mitigate stormwater run-off, and provide a more attractive environment. “Complete Streets”, or streets that take into account the needs of multiple users using multiple modes of transportation, should also be strongly encouraged. The following complete street and green street improvements should be implemented to the greatest extent feasible.

- In commercial areas, provide curb extensions or bump-outs at all crosswalks. Curb extensions reduce the overall length of a crosswalk, and extend the sidewalk into the street area, which makes crossing the street safer for pedestrians.
 - Landscaping of curb extensions should be encouraged.
- Travel lane widths for vehicles should be



Outdoor Dining, Hoboken NJ



Street Tree Planting Strip, Washington DC



Curb Extension, Mashpee Commons MA



Designated Bicycle Lane, Pottstown PA



Vegetated Stormwater Inlet, Philadelphia PA



Curb Inlet and Vegetation, Philadelphia PA

minimized to reduce the speed at which vehicles travel, and provide space for other amenities. This increases the safety of the streets for pedestrians, bicyclists, and other users.

- Landscaped medians should be included where space is available on major corridors such as Broadway and Madison Avenue.
 - Where street width is available, designated bicycle lanes should be striped into the street. This will help to encourage bicycling as a viable form of transportation, and reduce dependence on cars.
 - Where adequate street width is not available for designated bicycle lanes, signs or markings indicating that travel lanes shall be shared by both automobiles and bicycles should be posted.
 - Where adequate space is available, bus loading zones should be separated from travel lanes and parallel parking areas.
 - Shelters for passengers should be located at all bus stops along bus routes.
 - ADA accessible ramps should be provided at crosswalks at all intersections.
 - Green Stormwater infrastructure should be utilized to reduce the impacts of storms on the area storm-sewer system. Green stormwater infrastructure utilizes natural bio-filtration processes to collect water rather than channeling water directly to the storm sewer.
 - Curb inlets, or breaks in the curb line, should be provided to allow for some stormwater to infiltrate in areas where vegetation or a street tree is beside the curb. This can reduce the amount of water that flows to the sewer system.
 - Vegetated bio-swales along the sidewalk area should be provided where space is available. In lieu of simply a planter strip between the curb and the sidewalk, a vegetated bio-swale planted with vegetation that directly receives stormwater from the street should be encouraged.
 - Rain gardens should be encouraged in front yard areas or side yard areas of residences.
- The use of pervious paving materials for sidewalks and driveways shall be encouraged.
 - Pervious concrete
 - Grass-Crete
 - Interlocking pavers

LANDSCAPING STANDARDS

Landscaping is an important component of the overall streetscape environment, both in residential and commercial settings. Street trees, plantings, fences, and other landscaping can make the difference between an attractive streetscape, and a harsh one. The intent of the landscaping standards is to encourage appropriate landscaping in the form of street trees, shrubs, flowering plants, fences, and other features within the public and private realms of the streetscape.

Street Trees

- Street trees shall be placed within the required planting strip between the sidewalk and street, and spaced at a maximum of 50 feet on/center.
- No more than 50% of the trees on any single block may be the same species of tree.
- All street trees shall be planted within metal tree grates, or a system designed to allow for pedestrians to pass over the tree.
- Street trees shall be a minimum of 2.5 inches in caliper.
- Recommended street trees are:
 - Red Maple
 - Autumn Cherry
 - Okame Cherry
 - Red Oak
 - Little leaf Linden
 - Chinese Elm
 - Japanese Zelkova
- Trees may be located in the front yard area of a residential property if there is a setback of at least 6 feet.
- Front yard trees shall be a minimum of 2.5

inches in caliper.

- The following trees are recommended for front yard areas:
 - Shadblow Serviceberry
 - Eastern Redbud
 - Cherry Dogwood
 - Kousa Dogwood
 - American Dogwood
 - Common Witchazel

Shrubs and Plantings

- Placement of planters is encouraged in window sills of residences or stores, and near entrances to buildings.
- Wall mounted or floor mounted planters shall be located so that they do not hinder pedestrian movement along the sidewalk.
- Residential shrubbery shall be a minimum size of 3 gallon container, 18 – 24 inch B&B (balled & burlapped)
- Recommended residential shrubs:
 - Evergreen:
 - Dwarf barberry
 - Japanese holly
 - Blue holly
 - Juniper “Old Gold”
 - Japanese Pieris
 - Cherry Laurel
 - Nigra Arborvitae
 - Deciduous
 - Feather Reed Grass
 - Red Twig Dogwood
 - Burning Bush
 - Japanese Fothergill
 - Common Hydrangea
 - Oak Leaf Hydrangea
 - Winterberry



Street Trees, Alexandria VA



Metal Tree Grate



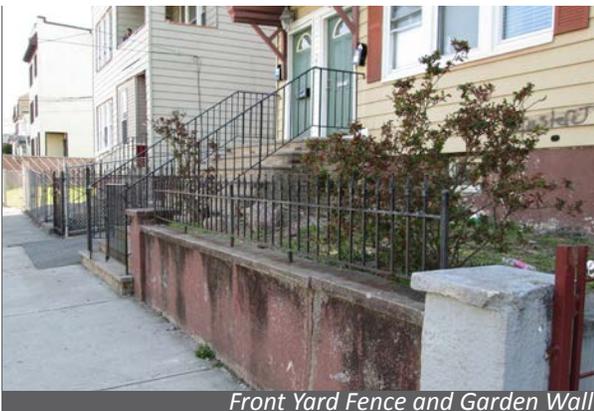
Street Plantings and Furniture



- Maiden Grass
- Switch Grass
- Fountain Grass
- Koreanspice Viburnum
- Groundcovers shall be a minimum size of 1 gallon container
- Recommended groundcovers:
 - Lillyturf
 - Japanese Garden Juniper
 - Blue Star Juniper
 - Pachysandra
 - Daylily
 - Hosta

Fencing

- Fences in a front yard area must be visually open, and no greater than 4 feet in height.
- Chain link fences are prohibited in the front yard area.
- Opaque Board on board fences are prohibited in the front yard area.
- Fences must be of wood or high quality material.
- Fences shall be no higher than 6 feet.



SIGNAGE STANDARDS

This section governs the number, type, and size of signs permitted on commercial buildings within the Fourth Ward Redevelopment Area. Signage is a vital piece of commercial business operations, as it signals to potential customers the type of products or services being offered. Attractive signage not only encourages business for that particular location, but contributes to the overall visual environment of the entire streetscape and neighborhood. Signage that is too large or too cluttered with information can overwhelm the visual realm of the streetscape. Signage should be coordinated with the architecture of the building, and with the context of the other surrounding buildings.

Signage Regulations

- No signs are permitted for residential properties other than signs indicating the street address.
- Commercial buildings shall be designed so as to include a “signage zone” integrated into the commercial storefront design. The signage zone shall be located above the ground level storefront.
- Commercial signage is permitted to have a maximum area of 10% of the ground floor street facing façade area, or 50 square feet, whichever is less.
- Each commercial use is permitted to have a maximum of two signs, the total area of which shall not exceed the maximum permitted sign area.
- Sign area shall be calculated as the smallest rectangle enclosing all design elements of the

sign.

- No sign shall project above the roofline of a building.
- Wall signs or building mounted signs shall not project further than 6 inches from the building façade to which they are attached.
- Blade signs shall be a minimum of 8 feet above the ground, and shall have a maximum area of 6 square feet.
- Blade signs may project no further than 3 feet from the face of the building to which they are attached.
- Window signs shall not be permitted to occupy an area greater than 20% of the surface of the window to which they are attached.
- Directional signage indicating the location of entrances, parking, or other vital information shall be permitted as deemed necessary to provide directions. Directional signage shall not exceed 4 square feet per sign.
- Signage on awnings is permitted to include text only, and no graphics.
- Awning signs are permitted to occupy a maximum of 20% of the surface area of the awning.
- Signs may be internally illuminated, or externally illuminated by gooseneck light fixtures.

Permitted Sign Types

- Wall Signs or Building Signs
- Blade Signs
- Window Signs
- Wall Murals (on existing blank walls only)

Prohibited Signs

- Freestanding Signs
- Ground mounted signs
- Flashing signs
- Off-premises signs



Blade Sign



Retail Wall Sign and Awning



Bicycle Parking, New Brunswick NJ



Parking Lot Buffer Plantings, Plainsboro NJ



Front Yard Parking, Not Permitted

PARKING AND CIRCULATION STANDARDS

The location and availability of parking within established commercial areas and residential neighborhoods is often very challenging. Parking must be provided for, but it must not overwhelm and degrade the pedestrian orientation of these areas. There will always be a limited amount of on-street parking and provisions must be required to provide off-street parking opportunities. Developers are required to provide on-site parking whenever possible, such as in new construction. However in situations, such as renovations of existing buildings, that do not contain parking spaces, developers are encouraged to develop off street parking arrangements in shared lots.

On-street parallel parking is provided on most of the streets within the Redevelopment Area, and may be taken into consideration in any development application.

The guidelines contained in this section of the Redevelopment Plan pertain to circulation and parking. The circulation and parking concerns of this Redevelopment Area were analyzed and compared with similar concerns in other Redevelopment Areas and the City as a whole.

Residential Off-Street Parking

- Residential driveways shall have a minimum width of 9 feet.
- Residential parking garages shall be located to the rear of the principal structure.
- Parking shall not be permitted in the front yard area of a residence.

Commercial Off-Street Parking & Circulation

- All 90 degree parking spaces shall have a minimum width of 9 feet, and a minimum depth of 18 feet where drive aisles are less than 24 feet in width.
- Where a drive aisle of at least 24 feet in width is provided, 90 degree parking stalls may have a width of 8.5 feet.
- All two-way drive aisles within parking areas shall have a minimum width of 22 feet.
- One-way drive aisles in parking areas shall have a minimum width of 12 feet.
- Parking lots shall be located no closer to a building than 3 feet.
- All surface parking lots that abut the public street shall be separated from the street by a buffer with a minimum width of 3 feet. Buffers shall include fencing, shrubberies, plantings, or a row of trees.
- Where multiple businesses are located in close proximity, shared parking arrangements should be encouraged.
- Driveways and curb cuts for surface parking lots should be minimized to reduce impacts to pedestrian circulation.
- No access driveway shall be located closer than 30 feet to another commercial parking lot access driveway.
- Bicycle storage racks should be located near the principal entrance to all commercial buildings.
- Bicycle storage should be provided at a minimum rate of 1 storage space for every 500 square feet of building area for commercial uses.

Landscaping, Lighting, and Pedestrian Access of Parking Areas

- Where a parking lot has 10 or more parking spaces, a minimum of 10% of the surface area shall be landscaped.
- Landscaping may be in the form of a hedge or shrubberies 3 feet to 4 feet in height, or a row of trees.
- Every parking lot shall provide a minimum of 1 tree for every 10 parking spaces. Trees may surround the parking lot or be located interior to the parking lot in tree islands.
- Parking lots that are visible from the public street shall not be enclosed by a chain link fence.
- Parking lots that have 30 or more parking spaces shall provide a pedestrian pathway or sidewalk of at least 5 feet in width through the parking lot area to public sidewalk, or to the primary building entrance.
- Pedestrian pathways/sidewalks within a parking lot shall be made of a different surface material than the parking areas of the lot, and are encouraged to include landscaping around the edges of the walkway.
- All parking lots shall be adequately illuminated to provide for the safety and security of users.



Parking Lot Landscaping, Asbury Park NJ



Landscaped Parking Lot, Pottstown PA



Landscaped Parking Lot, South Orange NJ



LIGHTING STANDARDS

This section governs street lights, buildings lights, and other fixtures that will provide lighting of the public realm and streetscape environment within the Fourth Ward Redevelopment Area. Public spaces must be adequately illuminated for the safety, security, and comfort of all people who may pass through them.

- Street light fixtures shall be mounted at a height of no greater than 18 feet. The supporting pole of the fixture shall have an overall height no greater than 20 feet.
- Street light illumination shall be consistent throughout a street area.
- The location of street lights shall be coordinated with the locations of street trees to avoid having trees block the lights.
- Building mounted lights shall be integrated into the overall architectural design of the building.
- All building entrances shall be illuminated by exterior lighting.
- Neon lighting is prohibited on commercial storefronts.
- Flashing lights are prohibited.



GREEN BUILDING STANDARDS

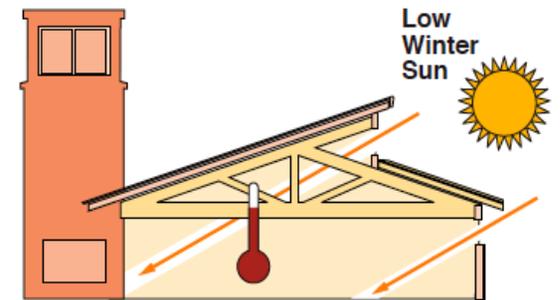
Residential and commercial buildings consume a significant amount of natural resources, primarily energy and potable water. Reducing the impacts of development through more efficient use of these resources can result in a cleaner community, as well as provide long-term cost savings for building owners. This section provides a list of recommended best practices for environmentally friendly ‘green’ building practices which are to be encouraged in all residential and commercial construction. The following are lists of some recommended best practices in green building from the New Jersey Housing and Mortgage Finance Agency (NJHMFA) and the U.S. Department of Housing and Urban Development (HUD). Not all standards will apply to all projects, however all standards and recommendations should be given due consideration in all projects for new construction or building renovations.

Site and Building Design

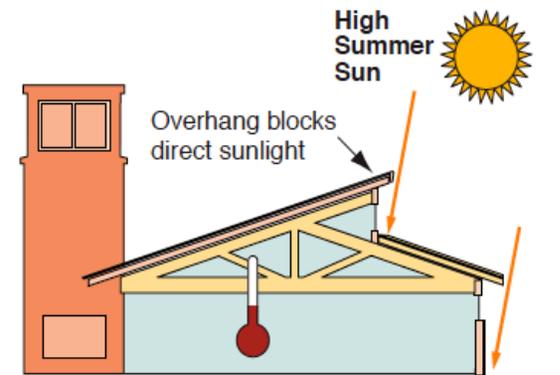
- Passive Solar, Ventilation & Shading Design
 - Passive Solar - The basic natural processes used in passive solar energy are the thermal energy flows associated with radiation, conduction and natural convection. When sunlight strikes a building, the building materials can reflect, transmit or absorb the solar radiation. These basic responses to solar heat lead to design elements, material choices and placements that can provide heating and cooling effects in a home. Passive solar energy means that mechanical means are not

employed to utilize solar energy.

- Solar Photovoltaic Readiness – Roof should be constructed to accommodate a future solar PV system. This includes ensuring that the roofing structure and finish material can bear the structure required to add a solar PV array. In addition, there must be a 2” (minimum) conduit run that provides roof-to-electrical panel room connectivity.
 - Passive Ventilation & Shading – Buildings and windows should be oriented to resist cold northern winds and lack of sun in the winter and open to warmer southern breezes in the summer. Apply suitable roof overhangs, awnings and/or deciduous trees.
- Bike Racks: Provide a safe and secure place to lock up bikes. Select an area that can be monitored via security, windows, and/or common passage.
 - Occupancy and Daylighting Controls: Lighting in community and meeting rooms, laundry, and other common spaces, should have occupancy and automatic daylight controls to reduce energy use when unoccupied. Common space does not include hallways, stairwells and any means of egress. For example, exterior porch and site lighting should have daylight sensors and controls.
 - Recycling Centers in Common Areas: Design buildings with easy access to recycling stations that are well marked, accessible, and easy to understand.
 - Air Ventilation Single-family: Install an in-unit ventilation system that complies with



South windows accept direct sunlight to light and warm the building interior



Passive Solar Heating (from Greenpassivesolar.com)

ASHRAE 62.2-2010.

- Air Ventilation Multi-family: Install apartment ventilation systems that satisfy ASHRAE 62.2-2010 and common area ventilation systems that satisfy ASHRAE 62.1-2010. Consider heat/energy recovery for 100% of corridor air supply.

Building Materials

- MERV 8 (or higher) air filters in ducted forced air systems: Install pleated furnace filters, minimum MERV 8, during testing and balanced of HVAC system and for the life of the system. If running ducted forced air system during construction, use MERV 8 filters during construction, replace regularly, and prior to system testing and balancing.
- Insulation with Low Formaldehyde Content: The most common form of insulation in homes today is fiberglass, fabricated primarily from silica sand, which is spun into glass fibers and held together with an acrylic phenol-formaldehyde binder. There are brands of fiberglass insulation that do not contain phenol-formaldehyde binding agents and are an unfaced white batt insulation bonded with a formaldehyde free thermosetting resin. The Uniform Construction Code prohibits urea-formaldehyde foam insulation. The binder used in batt insulation should be phenol-formaldehyde-free.
 - One of the more reasonable priced alternatives to fiberglass insulation is cellulose spray-in insulation; of which recycled newspaper is a major component. Other alternative insulation systems to consider are soy foam, recycled denim, and oyster shell insulation.
- Operable Windows: Choose windows that can be opened. Operable windows provide opportunities for natural heating, cooling, and ventilation as well as providing a direct connection to the outdoors and the neighborhood. Also, ensure that the window is easily operable. For example, if the window is too heavy to lift and does not have a lip to grab then it is not readily operable; this could be an issue for frail or elderly residents, who would have to call for assistance.
- Windows with Low-E coating: All windows installed should meet ENERGY STAR guidelines and have a low-E coating.
- ENERGY STAR Certified: All projects should use appliances and light fixtures that are ENERGY STAR certified, or equivalent.
- High Energy Factor Water Heaters: Install water heater with energy factor greater than 60% AFUE for gas fired units and 0.95 for electric. For unit-by-unit water heaters, use electric water heater (tank type) of 0.91 EF (efficiency) or greater; a natural gas water heater (tank type) of 0.60 or greater for 50-gallon, 0.62 EF or greater for 40-gallon, or 0.65 EF or greater for an instantaneous model (tankless).
- Easy to Use Programmable Thermostats: Provide a seven-day, digital programmable thermostat that runs on 24volts of the HVAC system, with battery backup, and no mercury. Make buttons large and system easy to use.
- Low-Flow Fixtures: Faucets should be a maximum of 1.5 gpm in the kitchen, and 1.0 gpm for the bathroom. Showerheads should be a maximum of 2.0 gpm.
- High Efficiency Toilets: Toilets shall have an efficiency of 1.3 gallons per flush, or better (less). Dual-flush toilets can also be used with a maximum flush of 1.3 gallons.
- Insulation Attic: For attics with closed floor cavities directly above the conditioned space, blow in insulation per manufacturer's specifications to a minimum density of 3.5 Lbs. per CF. For attics with open floor cavities directly above the conditioned space, install insulation to meet or exceed 2009 IECC levels.
- Insulation Flooring: Install \geq R-19 insulation in contact with the subfloor in buildings with floor systems over vented crawl spaces. Install a 6-mil vapor barrier in contact with 100% of the floor of the crawl space (the ground), overlapping seams and piers at least 6 inches.
- Radiant Barrier Roofing: Utilize radiant barrier sheathing or other radiant barrier material for roofs; if economically feasible, also use cool roofing materials.
- Sizing of Heating and Cooling Equipment: Size heating and cooling equipment in accordance with the Air Conditioning Contractors of America (ACCA) Manuals, Parts J and S, or ASHRAE handbooks.
- Domestic Hot Water Systems: Ensure the system(s) meet or exceed the efficiency requirements of ENERGY STAR for Homes' Reference Design. Insulate pipes by at least R-4.
- Composite Wood Products: Composite wood products should have all exposed edges and sides be sealed with low-VOC sealants.
- Environmentally Preferable Flooring: Use environmentally preferable flooring, including the Floor-Score certification. Any

carpet products used must meet the Carpet and Rug Institute's Green Label or Green Label Plus certification for carpet, pad, and carpet adhesives.

- Low/No VOC Sealants, Paints and Primers: All interior paints and primers must be less than or equal to the following VOC levels: Flats--50 g/L; Non-flats--50 g/L; Floor--100 g/L.
- Combustion Equipment: When installing new space and water-heating equipment, specify power-vented or direct vent combustion equipment.
- Mold Prevention-Surfaces: In bathrooms, kitchens, and laundry rooms, use materials that have durable, cleanable surfaces.
- Mold Prevention-Tub and Shower Enclosures: Use non-paper-faced backing materials such as cement board, fiber cement board, or equivalent in bathrooms.

Building Construction

- Ducts and HVAC protection: Completely seal duct and HVAC equipment openings with plastic film and tape, or other suitable material, until after final cleaning of unit. If system is used during construction, install MERV 8 filters on all return grills. In addition (or as an alternative), thoroughly flush and vacuum all ducts prior to system startup and upon completion of all construction and finish work.
- Combustion Devices Directly Vented or Sealed: With the exception of gas stoves, all combustion devices should be power vented or sealed combustion.
- Automatic Bathroom Ventilation: Install fans that directly vent to the outside in bathroom

with automatic timer control. This is also a minimum requirement for ENERGY STAR Certification. Fans should have a maximum of 1.5 sones (noise level).

- Direct Vent of Kitchen: All kitchen exhausts should be directly vented to the outside.
- Encapsulation of non-UF (Urea Formaldehyde) free composite cabinets: If Urea Formaldehyde is in any particleboard or other composite wood product incorporated into the interior of the project (cabinetry, countertops, etc.), all exposed edges (those not covered by another, sealing material – including backs) must be coated and sealed with water-based polyurethane or approved paint to slow the out-gassing rate of harmful toxins. Sealing can be done in shop before delivery; however, if the cabinetry is cut on-site, the cut faces must be resealed prior to installation.
- Exterior Wall Drainage Plane: Provide an exterior wall drainage plane using building paper, house-wrap or layered water resistant sheathings (rigid insulation or a foil covered structural sheathing) with seams taped or sealed.
- Window Flashing Details: All windows and exterior openings should demonstrate best practices for flashing details in order to create a weather resistant barrier. Details should be developed to meet the intent for both the window and wall system manufacturer's products. Window details should show pan and sill flashing, damming the edges of the bottom sill flashing, and location of weep holes to exterior facade.
- Air Sealing Building Envelope: Seal all accessible gaps and penetrations in the



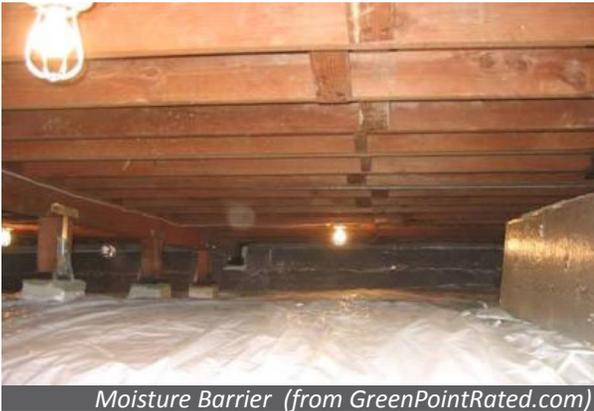
Kitchen Ventilation (from GreenPointRated.com)



Sealed Ducts (from GreenPointRated.com)



Low-Flow Faucet (from GreenPointRated.com)



Moisture Barrier (from GreenPointRated.com)



Mold Remediation (from USEPA)



building envelope with low-VOC caulk or foam.

- **Air Barrier System:** Ensure continuous unbroken air barrier surrounding all conditioned space and dwelling units. Align insulation completely and continuously with the air barrier.
- **Duct Sealing:** In buildings with ducted forced-air heating and cooling systems, seal all penetrations of the air distribution system to reduce leakage in order to meet or exceed ENERGY STAR for Homes' duct leakage standard.
- **Mold Prevention Water Heaters:** Provide adequate drainage for water heaters that include drains or catch pans with drains piped to the exterior of the dwelling.
- **Integrated Pest Management:** Seal all wall, floor, and joint penetrations with low-VOC caulking or other appropriate sealing methods to prevent pest entry.

Building Renovations

- **Lead-Safe Work Practices:** For properties built before 1978, use lead-safe work practices consistent with the EPA's Renovation, Repair, and Painting Regulation and applicable HUD requirements.
- **Radon Testing:** Since Paterson is located within the EPA's Zone 2, perform radon testing. For buildings with levels of radon greater than 4 pCi/L, implement EPA's recommended Residential Radon Mitigation Standard of Practice.
- **Mold Remediation:** Inspect the interior and exterior of the building for evidence of moisture problems. Document the extent

and location of the problems, and implement the proposed repairs according to the Moisture section of the EPA Healthy Indoor Environment Protocols for Home Energy Upgrades.

Other Green Building Recommendations

- **Smoke-Free Building:** Implement and enforce a "no smoking" policy in all common and individual living areas of all multi-family buildings. Common areas include rental or sales offices, entrances, hallways, resident services areas and laundry rooms.
- **Roof:** Roof warranty should be 30 years for pitched roofs and 20 years for flat roofs.
- **Recycle or salvage construction and demolition debris:** Develop a plan and protocol to properly sort and dispose of construction waste material separate from recycled material. Establish a system for daily collection and separation of materials designated to be recycled including concrete, metals, wood, recyclable plastics, bottles and cardboard, at a minimum.
- **Electric Charging Stations:** Electric Charging Stations may be provided within existing and proposed parking areas in public and/or private parking facilities.

CRIME PREVENTION THROUGH ENVIRONMENTAL DESIGN (CPTED) PRINCIPLES

Crime Prevention Through Environmental Design (CPTED) is a basic set of design principles that treat the physical environment as spaces that can either potentially enable or inhibit the likelihood that crimes will occur in those spaces. A key consideration of CPTED is making the streets and sidewalks safe for all users to get to their destinations. These principles should be viewed as an additional layer of design standards for all development applications. They should be applied to all reviews of public or private space, with particular emphasis placed on how a buildings’ design, landscaping, lighting, or other components may impact public safety, either perceived or real.

Natural Surveillance

Natural surveillance is the design of public spaces so that it feels as if someone may be watching, whether you can see them or not. People feel safer if they feel the presence of other people around. People are also less likely to commit crimes if they feel that they are being watched. This is why people are afraid of dark alleys at night. It makes them feel trapped and alone. Providing natural surveillance serves as a form of self-policing that functions as a crime deterrent, as well as giving people a sense of security.

- Buildings should be oriented towards the public street, with windows and doors facing the street, to create a sense of “eyes on the street”
- There should be a good visual connection between the public spaces of the street, and

ground level spaces of buildings on those streets. Blank walls should be avoided.

- The public realm should be visually open and clear of barriers. Any place where there is a potential hiding spot or an area that is out of view from the street or public areas creates a potentially hazardous and unsafe space.
- Landscaping such as shrubs or bushes should not create a visual barrier.

Lighting

Sidewalks, building entrances, and public spaces should be well illuminated at night. People feel safer if they can see what is ahead of them, and around them.

- Ensure all building entrances are well lit.
- The exterior of buildings along public streets and spaces should also be lit.

Maintenance and Code Enforcement

Simply conducting regular maintenance and repairs on a building or space, and keeping it clean and litter free, gives the impression that someone cares about that space, and has the means to control it. Areas that are dirty and show serious signs of dilapidation or disrepair indicate that there is little concern for the area, and that no one is asserting any ownership or control over the space. This leads to the impression that deviant or criminal behavior may be permissible in that space.

- Spaces should be designed to be low-maintenance, and easy to repair or clean.



Large opaque fence obstructs vision of pedestrians



Blank Wall facing Street



No “Eyes on Street”

ACTION PLAN

PATERSON FOURTH WARD



City of Paterson, NJ

Fourth Ward Redevelopment Plan

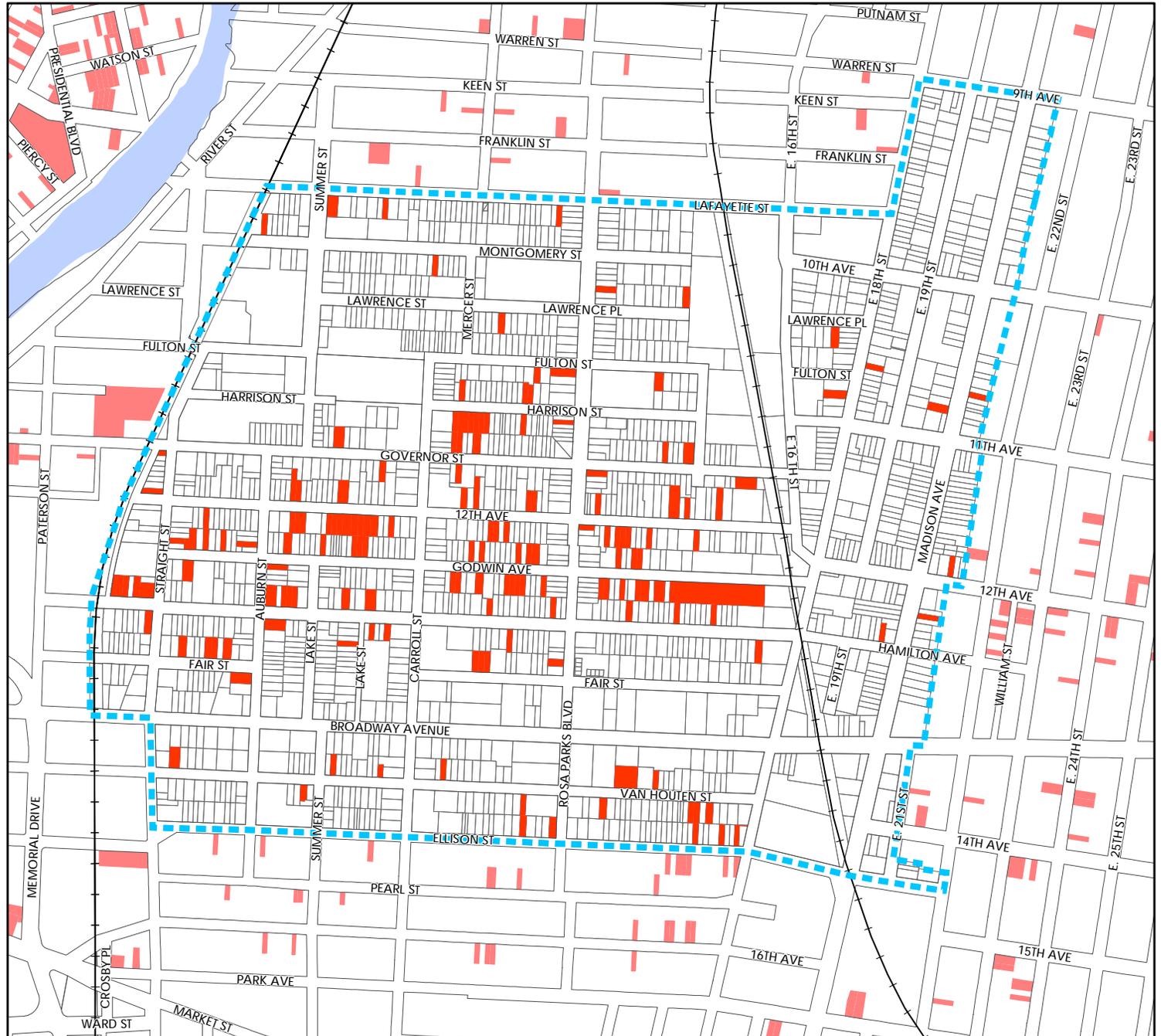
Vacant & Abandoned Properties

- Vacant & Abandoned Properties
- Fourth Ward Redevelopment Area



Source: NJGIN, NJDEP, NJOGIS, NJDOT, City of Paterson, 2014 MODIV Tax Assessment Data

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OPPORTUNITIES FOR STRATEGIC DEVELOPMENT

Vacant/Abandoned Properties List

Properties that are vacant or abandoned diminish the value of neighborhoods and can become havens for criminal and illicit activity. These properties are a burden on the City and should be aggressively pursued with actions to reduce their blighting impacts. The City's 2014 Master Plan identifies several steps that the City can take to address these properties, including identifying all of the properties that are vacant or abandoned, and aggressively taking code enforcement actions against these property owners.

The City has established an abandoned properties list. There are over 1,000 properties currently listed on the City's Vacant & Abandoned Properties database, which is regularly updated and maintained by the Division of Community Improvements. Many of these properties are within the Fourth Ward Redevelopment Area. The City Council in August 2014 adopted an ordinance (Ordinance #14-034) that requires that owners of properties on this list register their properties, and take actions to indicate they are not abandoning the property, or face stiff penalties. The ordinance also establishes procedures for either the City or another private party to acquire properties that have remained on the list, and are also delinquent in paying property taxes for a period of at least six quarters, through a tax sale. Essentially, if a property remains vacant for an extended period of time, the City can potentially acquire that property and put it to a productive use.

The City has been advancing its "Boards-Off" initiative to follow through on the 2014 ordinance. Each of these properties represents an opportunity for the City to transform a blighted property into an asset. There are many properties on the list scattered throughout the Fourth Ward Redevelopment Area, as is shown in the Vacant & Abandoned Properties Map. Most of these properties are small residential lots that are less than 5,000 square feet in area, although there are some larger parcels. Where multiple adjacent properties are abandoned, they could potentially be acquired together and developed as a larger opportunity site. If the City or a private developer is to acquire any of these properties, they could potentially be used in the following ways:

- Infill housing according to the standards of the zoning districts established by this Plan.
- Mixed Use developments with commercial space on the ground floor and residences or office space above.
- Smaller parcels that may not be suitable for residential or commercial development can be utilized as pocket parks, community gardens, or some other public space.

All Public/City Owned Properties

In addition to sites that the City may acquire either through the Vacant/Abandoned Properties list, there are a number of properties in the Fourth Ward Redevelopment Area that are already owned by either the City or another public or non-profit agency, such as the Paterson Housing Authority or the multiple faith-based organizations. While most of the properties in the Area that are owned by a



Vacant Property



Abandoned Property



City Owned Vacant Property

City of Paterson, NJ

Fourth Ward Redevelopment Plan

Publicly Owned Properties

-  Fourth Ward Redevelopment Area
-  Publicly Owned Properties



Source: NJGIN, NJDEP, NJOGIS, NJDOT,
City of Paterson, 2014 MODIV Tax
Assessment Data

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public agency are currently used as schools, public housing, or for other productive purposes, some of these properties can be potentially utilized for more than their current function. The City should consider the potential for mixed uses of their assets in the Area to help meet community needs. For example, public school properties can also serve as community centers when school is not in session.

Larger Development Opportunities

Where the Vacant/Abandoned Properties list provides opportunities for relatively small infill development of housing or small scale commercial establishments, there are a few sites within the Fourth Ward Redevelopment Area that are larger than a standard residential lot, and offer the potential for bigger development projects to catalyze development, and provide more visible impacts. These sites include the following properties, which are shown on the next pages:

Block 3202 Lot 10

This property, which is nearly two acres in size, sits adjacent to the NYS&W railroad tracks in the northeast end of the Area. The property has access from Montgomery Street and Lawrence Place at the ends of those streets, but is otherwise entirely surrounded by other properties, and has no real street frontage. It is currently clear of any development and is owned by the City.

This property is located within the RA-2 Residential District, and could potentially be developed with new multi-family dwellings or single-family townhouses. This property could also potentially be utilized as a new park for Area residents, providing additional recreational fields and playgrounds. The property is large enough to accommodate a number of uses in a single cohesive development plan.

If additional lots to the north and south of Lot 10 could be acquired and included in a larger development project, there is also a possibility of extending Jelsma Place to connect Governor Street to Lafayette Street, and provide street frontage to these lots.

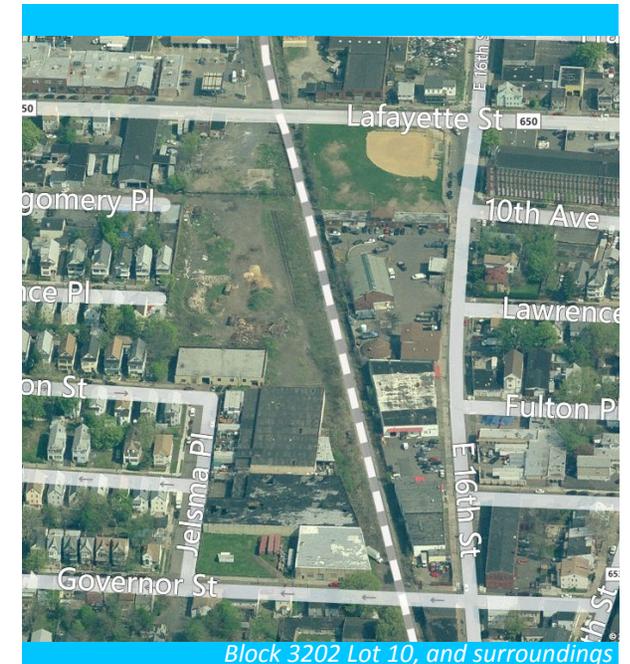


Block 3202 Lot 10

Block 3211 Lots 16-24

These properties, which together are 15,000 square feet (0.34 acres) in area, are on the corner of Rosa Parks Boulevard and Harrison Street in the center of the Redevelopment Area. The properties are all currently vacant of any structures, and are owned by the City’s Housing Authority. Adjacent to the properties are several homes that were recently constructed by local non-profit developers. This site presents an opportunity to continue the transformation of the block with new housing, and provide much needed quality housing options for residents.

These properties are located within the RA-2 Residential District of the Plan.



Block 3202 Lot 10, and surroundings

Block 3111 Lots 18-25.02

These properties, which are 13 adjacent lots and total approximately 28,000 square feet in area, all have frontage along Fulton Street near the intersection with Carroll Street. These lots are currently vacant and cleared. These properties would be ideal for development with new affordable housing.

Currently a 19-unit housing development is proposed for the site. The properties are located in the RA-1 Residential District of the Plan.



Block 3111 Lots 18 - 25.02

Block 3113 Lots 1 and 2

These two properties on Straight Street occupy nearly the entire block between Governor Street and Harrison Street. This is the site of the former Apollo Dye building, which was recently demolished, and is currently undergoing environmental remediation for soil contamination by the New Jersey Department of Environmental Protection. Once the remediation is complete on the 2.3 acre site, a new complex consisting of a 66-unit senior residence is proposed to be constructed.

This property is located within the Senior Residential district of the Redevelopment Plan.



Block 3113 Lots 1 and 2

Block 3507 Lot 14

This property on Godwin Avenue is a 1 acre lot that is developed with a 3-story brick mill building. The property is listed on the City's Vacant and Abandoned Property list, although the building appears to be in good condition. The property is currently owned by a not-for-profit organization, and could potentially be adaptively reused as loft style apartments.

This property is in the RA-2 Residential District of the Plan



Block 3507 Lot 14

Block 3422 Lots 13-18 & Block 3510 Lot 13

These properties make up two opportunity sites, which both have frontage on Broadway in the southeast corner of the Redevelopment Area. These properties are noted in the Madison Avenue Commuter Rail Corridor Study as opportunity sites for new mixed use development to support the transit oriented development goal of the Study. The proposed light rail stop is one block south of these properties, and if the Passaic/Bergen light rail project were to be implemented, this area of the City could see a transformation into a transit hub.

These properties having great proximity to the proposed light rail stop would be well suited for new multi-story mixed use development that would include retail uses on the ground level, and residences or office space above the retail. This would require the removal of the current uses of the properties, of which are occupied by a gas station, a fast food restaurant, and an auto-repair facility.

These properties are located within the GC General Commercial District, and the Transit Oriented Development District of the Redevelopment Plan.



Block 3422 Lots 13 - 18 and Block 3510 Lot 13

CIRCULATION IMPROVEMENTS

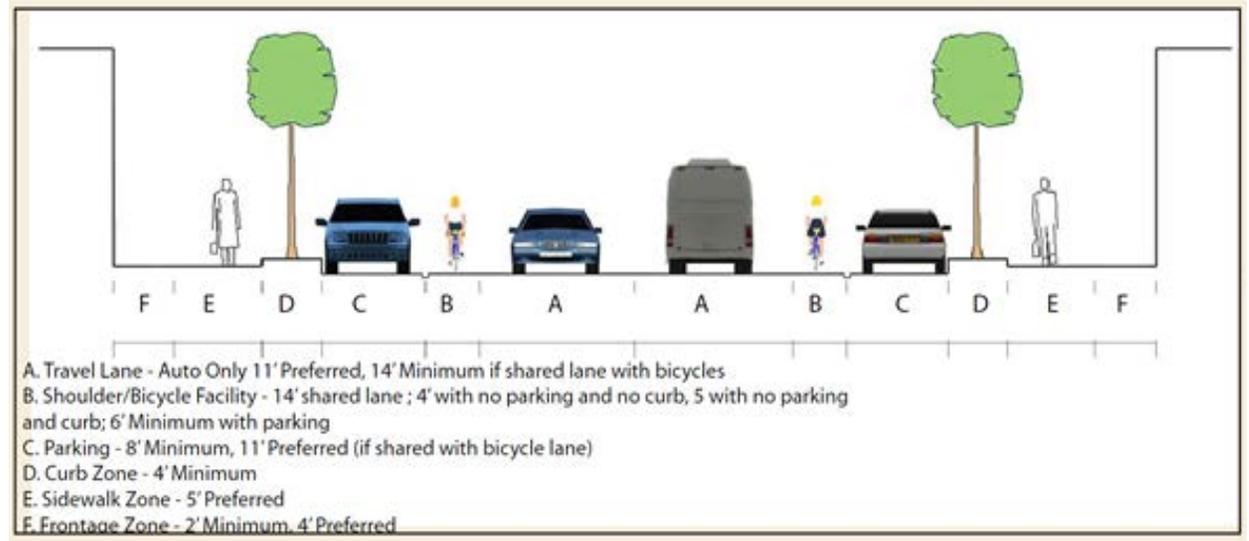
Passaic County Complete Streets

Passaic County is proposing a number of improvements to several of the County roads that are within the Fourth Ward Redevelopment Area. The proposed improvements are to transform the roads into Complete Streets, which are streets that are designed to accommodate a wide range of potential users, and not just automobiles. All proposed County Complete Streets within the Area are classified by the County as “Community Streets” within the Passaic County Master Plan Transportation Element.

A “Community Complete Street” in terms of the County’s Plan, is a street that is designed to connect local communities with one another, and connect to regional thoroughfares. A Community Street typically will be designed to include two travel lanes, one in each direction, and parallel parking on both sides of the street. Designated bicycle lanes may also be included within the street Right-of-Way. Special consideration will be given to street crossings and access to public transportation. The following county roads are proposed to receive Complete Streets treatments by the County:

- Lafayette Street (CR 650)
- East 18th Street (CR 653)
- Straight Street (CR 647)
- Madison Avenue (CR 649)
- 10th Avenue (CR 651)

These roads, which are some of the primary transportation thoroughfares in the Area and



Passaic County Community Street (from Passaic County Master Plan Transportation Element)

under the jurisdiction of the County, if improved in the ways described in the Passaic County Master Plan Transportation Element, can help to revitalize the Fourth Ward Redevelopment Area. Complete Streets along the primary commercial corridors will help provide access to and from businesses for local residents, as well as people beyond the boundaries of the Area.

Passaic County Green Streets

Along with a Complete Streets Approach to utilizing public Rights-of-Way for all potential users, the County is also pursuing a Green Streets initiative. A Green Street is one that incorporates low-impact design elements, and attempts to manage stormwater through natural biological processes, rather than simply channeling water through a sewer system.

Passaic County Bicycle and Pedestrian Priority Corridors

The County is also proposing to designate certain areas as bicycle and pedestrian priority corridors. These corridors will work in conjunction with their complete streets initiative, to prioritize travel for cyclists and pedestrians. The intent of these corridors is to create a cohesive network of bicycle and pedestrian infrastructure that connects important areas, such as Downtown Paterson, with their surrounding residential and commercial areas. Some streets would have dedicated bicycle lanes within the street, while others may have bicycle/pedestrian paths that run parallel to the street.

The following streets within the Fourth Ward Redevelopment Area are listed in the County Master Plan as designated bicycle and pedestrian priority corridors:

- Broadway (on-street)
- Madison Avenue (on-street)

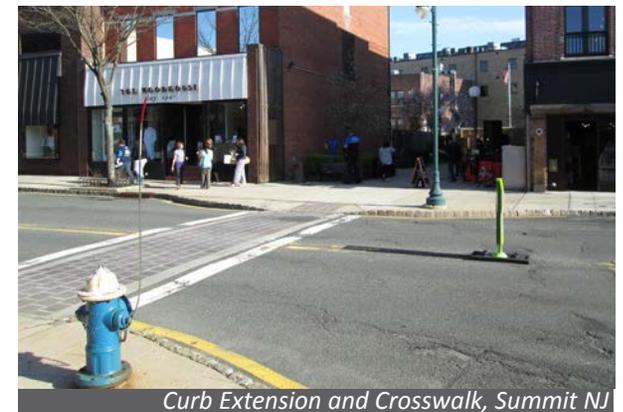
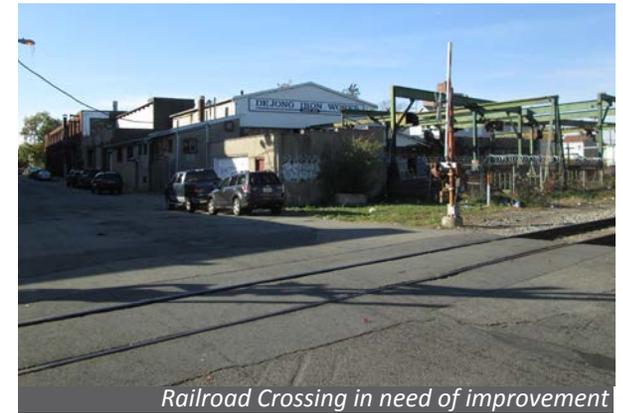
PROPOSED CIRCULATION IMPROVEMENTS

The proposed Green Streets and Complete Streets improvements on several of the County roads throughout the Area can significantly benefit the communities of the Fourth Ward if they are implemented. However, this still may leave some gaps in the circulation network where similar improvements can be made. The following circulation improvements can greatly help connect the Fourth Ward with other areas of the City:

- Target Broadway, which is a City thoroughfare and not a County highway, for similar Complete Streets treatment as the County roads. Broadway can use upgraded pedestrian crossings, as well as potentially designated bicycle lanes. This would help connect the Fourth Ward with Downtown, and the eastside of the City.
- Target Rosa Parks Boulevard for Complete Streets treatment. Another City thoroughfare, Rosa Parks Boulevard is a significant north/south connector within the Fourth Ward, and is also used as a bus route for several NJ Transit bus lines.
- Improved bus stops with shelters and designated bus stop lanes would also facilitate access to public transportation. This

can be done as a part of a complete streets approach to roadway design.

- Pedestrian amenities such as curb bump-outs and marked crosswalks should be added to high-traffic intersections. Many intersections already have marked crosswalks, however the paint is fading and difficult to see in some areas. Curb extensions or bump-outs can make crossing streets safer for pedestrians by reducing the length of street that needs to be crossed, and signaling to on-coming vehicles that there is a pedestrian crossing area ahead.
- In order to better connect the Fourth Ward with the surrounding communities in the City, improved connections across the railroad tracks will be needed. Where streets meet the railroad tracks, pedestrian paths should be constructed to enhance these crossings. Adequate street lighting would also be helpful to make crossing appear safer at night.
- Utilize the areas adjacent to the railroad tracks as potential bicycle and pedestrian trails. These railroad tracks run through the City, disrupting the vehicular circulation network, but they can also be used to help facilitate circulation through the City, by connecting all the areas along the tracks.



City of Paterson, NJ

Fourth Ward Redevelopment Plan

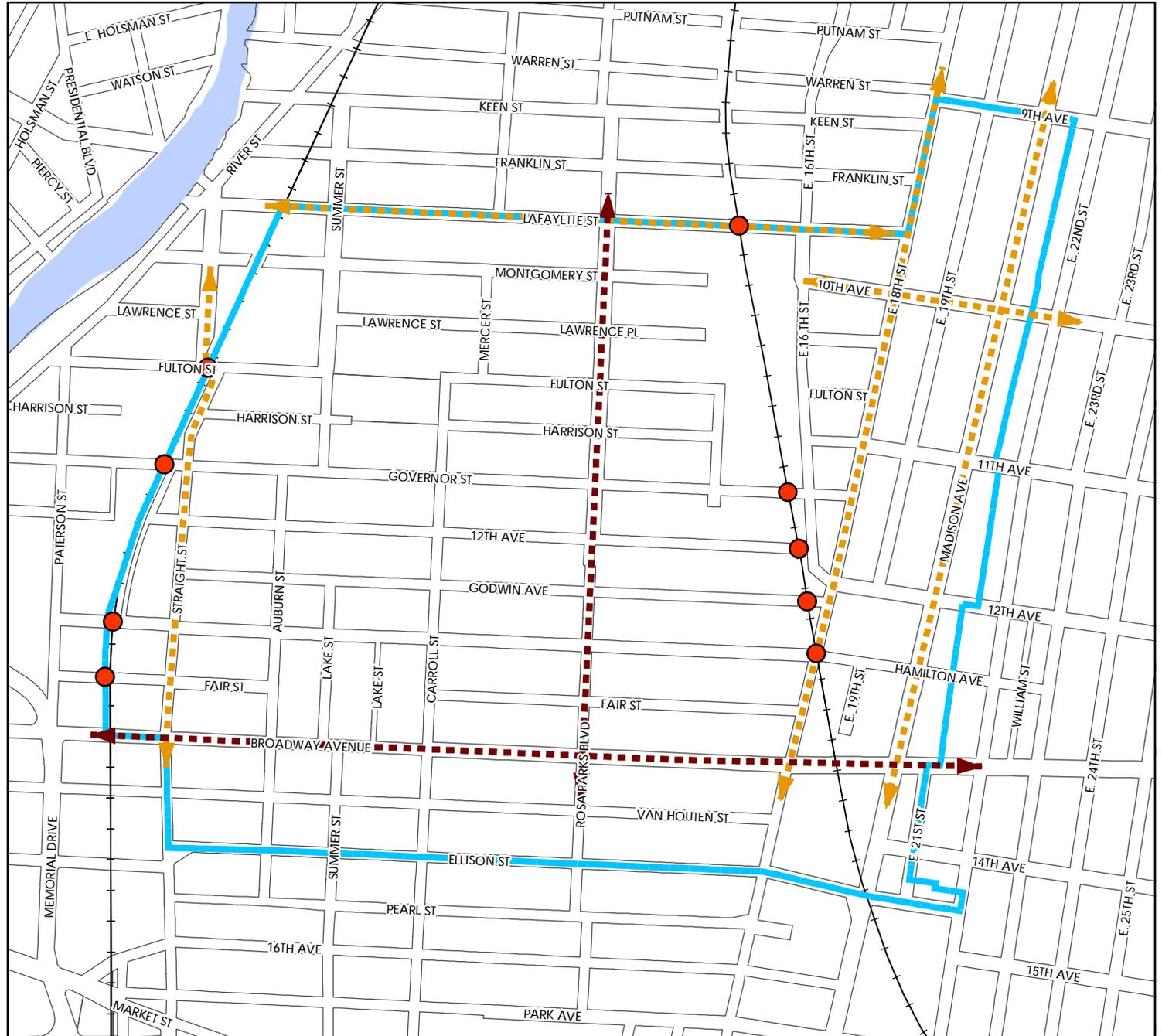
Proposed Circulation Improvements

-  Improved Rail Road Crossing
-  Proposed City Complete Streets
-  Proposed County Complete Streets
-  Fourth Ward Redevelopment Area



Source: NJGIN, NJDEP, NJOGIS, NJDOT, City of Paterson

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OPEN SPACE & RECREATION IMPROVEMENTS

The Fourth Ward Redevelopment Area is in need of greater access to open space and recreation areas. Currently, there are relatively limited resources available within the Area. Wrigley Park and the 12th Avenue Playground provide some outdoor recreation opportunities, as does the Putnam Oval, in the form of several basketball courts, a baseball/softball field, and playground equipment. The spaces within the Area that provide recreation opportunities, are located in the eastern and northern ends of the Area, leaving the southwestern end of the Area closer to the Downtown, without adequate access to recreation space.

Opportunities for Recreation

Straight Street at Hamilton Avenue and Fair Street

There are currently a number of properties along Straight Street near Hamilton Avenue and Fair Street in the southwest end of the Area which are vacant, abandoned, or underutilized. These properties, which are primarily narrow residential lots, could potentially be consolidated to create larger areas that would range from 8,000 square feet to over 20,000 square feet in size. Areas of this size could be potential parks to meet the needs of Area residents. These properties should be given consideration to be acquired by the City and developed as new recreation sites.

Block 3202 Lot 10

This 1.9 acre property which is adjacent to the railroad tracks, which was noted as an opportunity site for residential development, has ample space to potentially be used to provide outdoor recreation opportunities in addition to any residential development. While this property does present an opportunity to potentially increase available recreation space in the Fourth Ward, its location near both Putnam’s Oval and Wrigley Park, is an area of the community that already has easy access to parks and open space.

Block 4202 Lots 3, 22, 23

These three properties, which are located just east of the intersection of Broadway and Rosa Parks Boulevard in the Area, are currently vacant, and could potentially be utilized as a small park. In total they are over 29,000 square feet in size, and have frontage on both Broadway and Van Houten Street, giving them adequate access from two public streets. These properties have the space available to be used for numerous recreation purposes, such as providing playgrounds, a basketball court, or other such space.



OPPORTUNITIES FOR HOME OWNERSHIP

The City should actively pursue increasing the levels of home ownership within the Fourth Ward community. While any form of tenure may be permitted within the Redevelopment Area, providing new opportunities for home ownership should be encouraged. Raising the levels of home ownership in the Area where the vast majority of residents are renters should be viewed as a substantial component of the overall revitalization and redevelopment of the Area. Raising the levels of homeownership in the Area should be done by connecting residents to the resources available to promote homeownership, and assist first time home buyers. These resources include:

- State resources from NJHMFA
- Federal resources from the Department of Housing and Urban Development (HUD)
- Specific programs are discussed in the funding opportunities section of this Plan

RELATIONSHIP TO OTHER PLANS

PATERSON FOURTH WARD



RELATIONSHIP TO OTHER PLANS

This updated Redevelopment Plan has been prepared as the direct result of the City of Paterson’s Master Planning Process. As a result, many of the goals and objectives of this Plan are in direct compliance with the City, County, and State Planning documents. The following is a brief overview of the appropriate planning documents of regional significance, and how this Plan relates to those documents.

2014 PATERSON MASTER PLAN

The Planning Board of the City of Paterson adopted a new Master Plan in April of 2014 which provides a comprehensive guide for the development of the City. The Master Plan includes several goals and objectives that have relevance to the Redevelopment of the Fourth Ward Area.

Land Use Goals

- To address the problems associated with vacant and abandoned properties
 - Convert vacant and abandoned properties to occupied properties on the tax rolls
 - Encourage adaptive reuse of obsolete buildings
- To promote infill housing development
- To improve the viability of the City’s neighborhood commercial corridors
- To address outdated or expired redevelopment plans
- To develop targeted opportunity sites that will serve as catalysts and symbols of the revitalization of the City of Paterson

Urban Design Goals

- To promote and implement appropriate and attractive design standards for new construction
- To improve the appearance of the City’s streets

Housing Goals

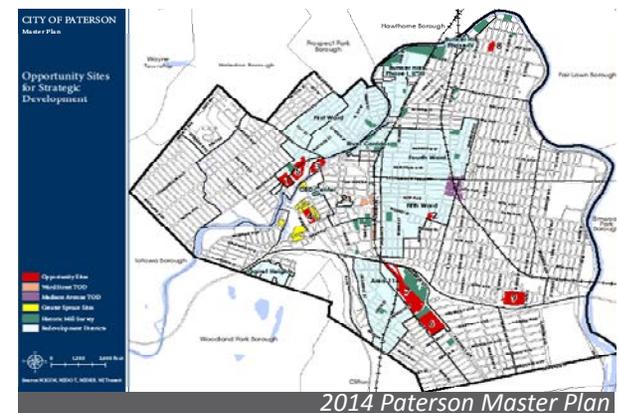
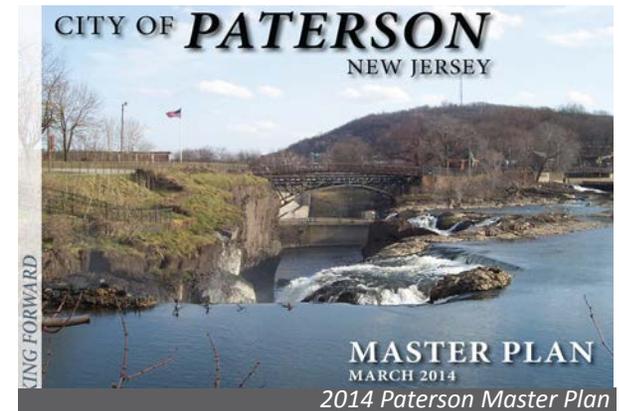
- To increase the supply of affordable housing
 - Provide affordable housing options amongst mixed income neighborhoods
- To provide opportunities for the construction and/or rehabilitation of affordably priced market rate housing
 - Consider conversion of vacant industrial buildings to residences
 - Promote infill housing development on vacant properties
- To stabilize and improve existing residential neighborhoods

Community Facilities Goals

- To provide facilities to meet the needs of residents of all ages, in each neighborhood
 - Address the need for libraries or community resource centers

Economic Development Goals

- To promote the development of vacant and abandoned properties
- To provide opportunities for local employment growth





Vacant Property



Wrigley Park



Business on Broadway

Historic Preservation Goals

- To encourage the restoration and/or adaptive reuse of historic structures in the City, such as the historic mills.

Recreation/Open Space Goals

- To provide increased access to open space for City residents
- To provide more opportunities for active and passive recreation throughout the City

The Master Plan also made several specific recommendations regarding the Fourth Ward Redevelopment Area:

- Permit multi-family housing in the STF-2 (RA-2) district
- Acquire properties throughout the designated Area and redevelop them with appropriate housing or commercial uses
- Enforce the recommended residential and commercial design standards

STATE PLANS

State Development and Redevelopment Plan (SDRP)

In 2001, the State Planning Commission adopted the State Development and Redevelopment Plan (SDRP). The SDRP is a document that, while not binding, guides State-level development and redevelopment policy as well as local and regional planning efforts. The Plan includes eight statewide goals and dozens of policies which are intended to implement the goals. The goals are as follows:

1. Revitalize the State’s cities and towns.
2. Conserve the State’s natural resources and systems.
3. Promote beneficial economic growth, development and renewal for all New Jersey residents.
4. Protect the environment, prevent and clean up pollution.
5. Provide adequate public facilities and services at a reasonable cost.
6. Provide adequate housing at a reasonable cost.
7. Preserve and enhance areas with historic, cultural, scenic, open space, and recreational value.
8. Ensure sound and integrated planning and implementation statewide.

The SDRP also includes a State Plan Policy Map, which divides the state into regions known as Planning Areas. The Map also identifies ‘centers’, locations into which development is to be directed. The City of Paterson is located within Planning

Area 1, or the Metropolitan Planning Area, and is designated as one of 8 urban centers of the SDRP.

This Plan meets stated goals 1, 3, and 6 in particular of the SDRP.

- The intended purpose of this Plan is to revitalize certain portions of the City of Paterson, a recognized urban center by the SDRP.
- New and improved housing is a central aim of this Plan.
- This Plan promotes beneficial economic growth for residents of the City.

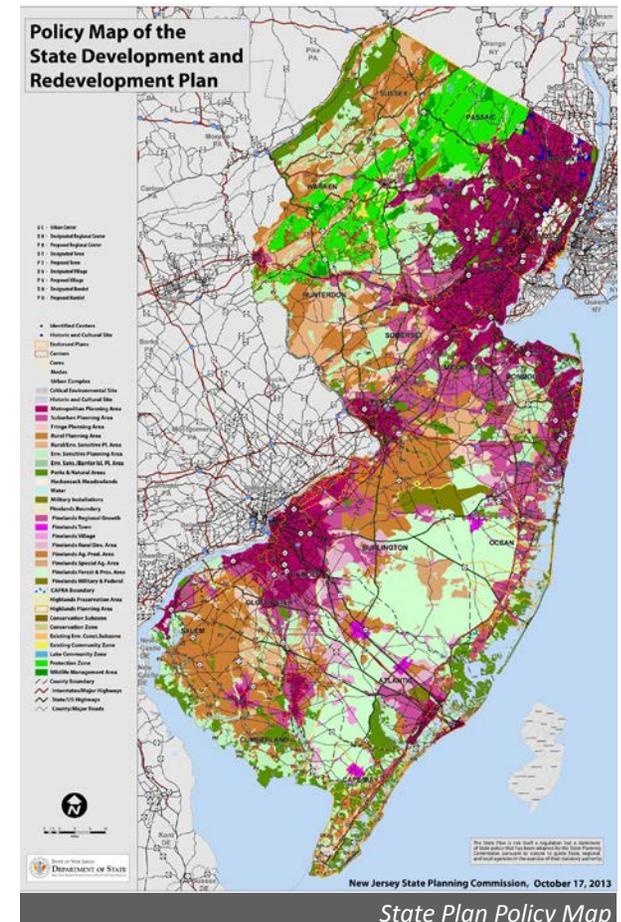
- Garden State Value #1 Concentrate development and mix uses
- Garden State Value #2 Prioritize Redevelopment, infill, and existing infrastructure
- Garden State Value #3 Increase job and business opportunities in priority growth investment areas

State Strategic Plan

The final draft of the State Strategic Plan was last released in 2011. While the State Strategic Plan has not been officially adopted, and the SDRP is still the official State Plan, it is still prudent to review the relationship between this Plan and the State Strategic Plan Draft. This Plan is consistent with the stated goals of the State Strategic Plan. Specifically this Plan exemplifies the guiding principle of “Spatial Efficiency”, where new development is encouraged to be located in areas where infrastructure is already in place to support growth.

This Plan meets the following goals of the State Strategic Plan:

- Targeted economic growth of industries of statewide and regional importance, with healthcare being one of the targeted industries.
- Major urban centers are to be targeted as priority growth investment areas



PASSAIC COUNTY PLANS

Passaic County Master Plan Land Use Element

The County Master Plan was last updated in April 1988 when the Land Use Element was added. The following are some of the goals of the “Land Use Plan”, that are in tune with the goals and objectives of this Redevelopment plan:

- **Balanced Development:** Provide for balanced development of the County with various types of residential uses, and with non-residential uses to support them; to encourage and maintain a balance between jobs and housing so that workers can find suitable housing within the county.
- **Concentrate Development and Conserve Critical Lands:** Concentrate development in areas with existing infrastructure in order to promote efficient transportation, conserve energy and reduce pollution; Encourage the location of new facilities; Encourage the location of new facilities and services near housing and jobs.
- **Business and Industry:** Provide for commercial and industrial areas sufficient to maintain a viable economic base for the County.

Passaic County Master Plan Transportation Element

A primary focus of the Transportation Element of the County’s Master Plan, adopted in Fall 2012, is on implementing “Complete Streets” on many county thoroughfares. A Complete Street is a street that has specific improvements intended to

provide for the safe and efficient transportation of many differing users using multiple different modes of transportation. This would include provisions for automobiles, bicycles, pedestrians, buses, and wheelchairs, all potentially using the same public right-of-way as a safe and suitable means of transportation.

The County Plan identifies several different classifications of Complete Streets, and as discussed previously, several streets within the Fourth Ward Redevelopment Area are proposed to be “Community Complete Streets”.

- Lafayette Street (CR 650)
- East 18th Street (CR 653)
- Straight Street (CR 647)
- Madison Avenue (CR 649)
- 10th Avenue (CR 651)

Passaic County Master Plan Parks, Recreation, and Open Space Comprehensive Plan Element

The County’s Parks and Open Space Plan Element was recently adopted in July of 2014, and includes a new vision for County operated parks, and several goals and objectives that have relevance to the Fourth Ward Redevelopment Area of the City of Paterson:

- Expand the Passaic County Park System
 - Acquire new properties in strategic locations, such as along rivers, within historic and scenic byways, in areas that are adjacent to existing areas of the Passaic County Park System, and in historically underserved

municipalities.

- Partner with municipal, state, and local government, as well as private organizations to acquire and maintain new properties.

There are currently no County parks within the Fourth Ward Redevelopment Area.

Passaic County Future, the Passaic County Sustainability Plan

In May of 2013, the County adopted a Sustainability Element of their Master Plan. The Sustainability Element is a comprehensive approach to achieving sustainability in Passaic County, and includes a number of recommendations that are consistent with the aims of this Plan.

- Encourage mixed use, and live/work development and redevelopment.
- Improve sidewalk conditions and street crossing amenities along existing or potential pedestrian corridors.
- Encourage new housing options through adaptive reuse and infill development.
- Increase access to and availability of developed green space for active and passive recreation.

PLANS OF ADJACENT MUNICIPALITIES

The Master Plan for the Borough of Prospect Park

The Fourth Ward Redevelopment Area is encompassed entirely by other lands within the City of Paterson, and is not adjacent to any other municipality.

OTHER STATUTORY REQUIREMENTS

PATERSON FOURTH WARD



OTHER STATUTORY REQUIREMENTS

Property to Be Acquired

No property acquisition through the use of eminent domain is authorized or anticipated as a part of this Plan.

The primary means of property acquisition for the City will be through the Vacant and Abandoned Properties List, and the authority possessed by the City for acquiring properties that have been determined to be abandoned through the procedures outlined in Ordinance # 14-034.

Relocation Assistance

It is anticipated that the designated redevelopers will address any relocation needs through acquisition of parcels. The City of Paterson, however, will provide all displaced tenants and landowners with the appropriate relocation assistance, pursuant to applicable State and Federal law, should relocation be necessary. Such assistance will be provided through an appropriately designated office which will assist in any relocation of persons, businesses or other entities. The local housing market, including Paterson and surrounding communities, contains an ample supply of comparable replacement housing to absorb the residents of Paterson who may be displaced by the redevelopment process. Further, Paterson and the surrounding area contains sufficient land and buildings which would be appropriate for relocation of existing businesses from the Redevelopment Area. If relocation is not directly caused by the Redevelopment Plan, the City assumes no responsibility for relocation of people and businesses.

Affordable Housing

No affordable housing units are identified to be removed as part of the implementation of this Redevelopment Plan. Any affordable housing obligation incurred by a redeveloper shall be addressed through a Redeveloper Agreement.



Housing in the Fourth Ward



Housing in the Fourth Ward



Fourth Ward Redevelopment Area

IMPLEMENTATION

PATERSON FOURTH WARD





Wrigley Park



Available Land in the Fourth Ward



Public Art in the Fourth Ward

ADMINISTRATIVE PROCEDURES

Redevelopment Entity

The City Council shall serve as the Redevelopment Entity.

Amending the Redevelopment Plan

Upon compliance with the requirements of applicable law, the City Council of Paterson may amend, revise or modify this Redevelopment Plan, as circumstances may make such changes appropriate.

Duration of the Redevelopment Plan

This Redevelopment Plan, as amended, shall be in full force and effect for a period of thirty (30) years from the date of approval of this Plan by the City Council.

Conveyance of Land

The City Council may sell, lease, or otherwise convey to a redeveloper for redevelopment, subject to the restrictions, controls and requirements of this Redevelopment Plan, all or any portion of the land within the Redevelopment Area which becomes available to disposal by the municipality.

Redevelopment Agreements

The following restrictions and controls on redevelopment are statutorily imposed by the Local Redevelopment and Housing Law, in connection with the selection of a redeveloper or redevelopers and shall apply notwithstanding the provisions of any zoning or building ordinance or

other regulations now or hereafter in force. The restrictions and restraints shall be implemented by appropriate covenants or other provisions in redeveloper agreements and/or disposition instruments.

- The redeveloper(s) will be obligated to carry out the specified improvements in accordance with the Redevelopment Area Plan.
- The redeveloper(s), its successors or assigns shall devote land within the Redevelopment Area to the uses specified in this Redevelopment Plan.
- The redeveloper(s) shall begin the development of said land for the use(s) required in this Redevelopment Plan within a period of time that the City Council fixes as reasonable.
- Until the redeveloper(s) completes construction of the improvements, the redeveloper(s) will not be permitted to sell, lease, or otherwise transfer or dispose of property within the Redevelopment Area without prior written consent of the City.
- Upon completion of the required improvements, the conditions determined to exist at the time the Redevelopment Area was determined to be in need of redevelopment shall be deemed to no longer exist, and the land and improvements thereon shall no longer be subject to eminent domain as a result of those determinations.
- No covenant, agreement, lease, conveyance or other instrument shall be effected or executed by the redeveloper(s), the City Council, or the successors, lessees, or assigns of either of them, by which land in the Redevelopment Area is restricted as to

sale, lease, or occupancy upon the basis of race, color, creed, religion, ancestry, national origin, sex or marital status.

- Neither the redeveloper(s) nor the City Council, nor the successors, lessees, or assigns shall discriminate upon the basis of race, creed, religion, ancestry, national origin, sex or marital status in the sale, lease or rental or in the use and occupancy of land or improvements erected or to be erected thereon, or any part therein.



Carroll Street



Housing in the Fourth Ward

FUNDING OPPORTUNITIES

The redevelopment of the Fourth Ward Area will require a mixture of private and public funding. Areas within adopted Redevelopment Plans qualify for several sources of government funding. These funds should be used to leverage private investment. The following is a listing of the most relevant current potential sources of funding for community improvements along with a brief description of those programs. The City already participates in many of these programs. This list is not intended to be exhaustive. There are numerous other sources of funding available and new options may become available in the future that should be considered.

Federal Programs

- Community Development Block Grants (CBDG) – Community Development Block Grants are a department of Housing and Urban Development (HUD) program that provides a range of housing and community development activities, but must meet one of the program’s objectives of benefiting low and moderate income families, preventing or eliminating blight, or addressing emergency needs.
- HOPE VI Revitalization Grants – HOPE VI is a HUD funded grant that can be used to rehabilitate or transform distressed public housing developments. Funds are given to projects that will help to enable self-sufficiency in public housing residents and de-concentrate urban poverty. The new mixed income housing project at the former Alexander Hamilton Homes site was funded

through a HOPE VI grant.

- Section 8 Voucher Program – Another HUD program that is administered through the Paterson Housing Authority, Section 8 vouchers provide subsidies for low income families to be used for direct housing rental assistance.
- Home Investments Partnership Program (HOME) – The HOME program provides grants to communities specifically for the development of affordable housing for low income households. HOME funds are awarded annually as grants to participating municipalities, in the form of trust funds.
- USEPA Brownfields grants – The Environmental Protection Agency provides several grant and revolving fund loan programs to aid in the assessment and clean-up of brownfield sites.

State Programs

- NJEDA Economic Redevelopment and Growth Grant (ERG) – The ERG program provides financial assistance to help with gap financing for development projects which have secured traditional debt and equity financing, but fall short of total development costs, creating a small financing gap. Tax credits or grant reimbursements are available for up to 20% of total development costs for approved projects. Projects in Paterson may qualify for additional funding, up to 40% due to the City being identified as a distressed municipality in a growth zone.
- NJEDA Grow NJ program – This program, which provides financial incentives for companies to relocate and expand in New

Jersey. The City of Paterson is a ‘qualified incentive area’ and therefore is able to receive companies using the funds from this program. The program provides both grants and insurance tax credits for creating or retaining jobs.

- NJEDA Fund for Community Economic Development – This program provides loans to other institutions to fund small business loans and economic development initiatives in urban communities such as Paterson. Direct loans for finance gaps in development projects are also available through this program.
- NJEDA Angel Investor Program – The Angel Investor Program provides tax credits of up to 10% of a qualified investment up to \$500,000 for investments in technology businesses that employ less than 225 persons.
- NJEDA Brownfields and Contaminated Site Remediation Program – The NJEDA Brownfields program provides funding for brownfields remediation through new sales taxes that are derived from new businesses located on a former brownfield site. Developers can qualify for up to be reimbursed for up to 75% of the costs of remediation efforts of the site.
- NJDEP Green Acres – The Green Acres program uses State funds to provide matching grants to non-profit organizations, and grants and loans to municipalities to fund the acquisition of property to be used as outdoor recreation parks or conserved open space. This includes athletic fields, running trails, and playgrounds.
- New Jersey Environmental Infrastructure Financing Program – NJEIT provides a

variety of financing and loan options for many different environmental infrastructure projects, such as stormwater management, combined sewer remediation, wastewater treatment, brownfields remediation, and projects to address clean drinking water standards.

- NJDOT Bikeways – The NJ Department of Transportation provides grants to counties and municipalities to create new bicycle infrastructure to promote bicycling as a viable form of transportation. Bicycle paths and dedicated bicycle lane projects are given priority funding.
- NJDOT Safe Streets to Transit (SSTT) – This is a project by the NJDOT to assist municipalities in increasing accessibility of public transportation services. Grants may be provided for projects such as intersection safety improvements, new sidewalks, traffic control devices, and pedestrian signals that are within 1 mile of a mass transit stop.
- NJHMFA Live Where You Work – This is a program that provides assistance to first time home buyers and others in eligible urban target areas who wish to purchase a home and live in the town where they work.

environments. Funds for the OSFPT come from an additional property tax that is levied against all properties in the County to provide a general fund.

- Passaic County Improvement Authority (PCIA) Financing Programs – PCIA provides redevelopment bonds for projects, and financing options for many of the tax credit programs offered by the State. PCIA also offers municipal and non-profit financing programs for capital projects and other improvements, including schools.

Passaic County Programs

- Open Space and Farmland Preservation Trust Fund (OSFPT) – The County's OSFPT provides funds to municipalities or non-profit conservation agencies to acquire or preserve land or water areas that have a natural, scenic, or open condition. Lands acquired or preserved can be used for recreation purposes, or as preserved natural