

# CITY OF PATERSON



PREPARED BY:  
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## FIRST WARD REDEVELOPMENT PLAN

CITY OF PATERSON, NEW JERSEY



# November 2017



# ACKNOWLEDGEMENTS

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# INTRODUCTION

## PATERSON FIRST WARD





## INTRODUCTION

Paterson's First Ward on the north side of the Passaic River, is a 200+ acre area of the City which has been plagued by issues of neglect and abandonment for many years. The First Ward Neighborhood between the Passaic River, North 7th Street, West Broadway, and Totowa Avenue, is in need of new investment and revitalization. The City of Paterson for many years has had a history of using Redevelopment as a tool for proactively planning the City's future, and stimulating new development in neighborhoods that need improvements. The First Ward Redevelopment Plan serves as a guide for new development in the Northside of the City.

Redevelopment as a planning tool for municipalities such as Paterson provides a number of benefits. Redevelopment allows the City to take a more proactive approach to development, and attract the right type of development it seeks to benefit the neighborhoods. Redevelopment also provides financial incentives for potential developers that may not be available otherwise. Developers can qualify for long-term tax abatement and low-interest loans to offset the costs of development, which can help stimulate growth where it otherwise may not occur.

This Plan is an update to the First Ward Redevelopment Plan. Since the original adoption of the First Ward Redevelopment Plan in October of 2003, the following significant events have taken place, which have impacted the First Ward Area:

- **Economic Recession**

The national economic recession that began

in 2008 and resulting housing foreclosure crisis had a major impact on the City of Paterson, including many properties in the First Ward. This has contributed to the high number of residences in the Area that are either boarded up or abandoned.

- **Flooding events of Hurricane Irene and Super-storm Sandy**

Recent storm events that brought heavy rainfall to the region have resulted in devastating floods that left many homes and businesses in the Area damaged.

- **Development of a Northside Neighborhood Plan**

A revitalization plan for the Northside neighborhood, which occupies the areas east of West Broadway, and south of 7th Street within the First Ward Redevelopment Area, was developed in order to provide for economic development opportunities, and quality of life improvements for residents.

- **Development of a New Master Plan for the City**

The City adopted a new Master Plan in April of 2014 which updated the goals and objectives from the previous Master Plan. The new Master Plan also made specific recommendations for the First Ward Redevelopment Area.

- **Paterson Great Falls National Historical Park**

The National Park Service has recently adopted a General Management Plan for the National Historical Park.



First Ward Redevelopment Area



First Ward Redevelopment Area



First Ward Redevelopment Area



*Abandoned Building*



*Homes and Businesses in the First Ward*



*Historic Mill in the First Ward*

## CONTENTS OF A REDEVELOPMENT PLAN

The Redevelopment Plan becomes the formal planning document guiding the redevelopment and revitalization of the Redevelopment Area. The Redevelopment Plan is statutorily required to include an outline for the planning, development, redevelopment or rehabilitation of the Redevelopment Area sufficient to indicate:

1. Its relationship to definitive local objectives as to appropriate land uses, density of population and improved traffic and public transportation, public utilities, recreational and community facilities and other public improvements.
2. Proposed land uses and building requirements in the redevelopment area.
3. Adequate provision for the temporary and permanent relocation, as necessary, of residents in the redevelopment area that will be displaced including an estimate of the extent to which decent, safe and sanitary dwelling units affordable to displaced residents will be available to them in the existing local housing market.
4. An identification of any property within the Redevelopment Area that is proposed to be acquired in accordance with the Redevelopment Plan.
5. Any significant relationship of the Redevelopment Plan to:
  - a. The Master Plans of contiguous municipalities;
  - b. The Master Plan of the County in which the municipality is located; and
  - c. The State Development and Redevelopment Plan adopted pursuant to

the "State Planning Act" P.L. 1985,C.398 (N.J.S.A.52:18A-196 et seq.).

6. As of the date of the adoption of the resolution finding the area to be in need of redevelopment, an inventory of all housing units affordable to low and moderate income households, as defined pursuant to section 4 of P.L. 1985 c.222 (C.52:27D-304), that are to be removed as a result of implementation of the redevelopment plan, whether as a result of subsidies or market conditions listed by affordability level, number of bedrooms, and tenure.
7. A plan for the provision, through new construction or substantial rehabilitation of one comparable, affordable replacement housing unit for each affordable housing unit that has been occupied at any time within the last 18 months, that is subject to affordability controls and that is identified as to be removed as a result of implementation of the redevelopment plan.

This Redevelopment Plan provides a detailed guide for the revitalization of this area of Paterson. The following is a review of the area, the basis for the development of the Plan in the Master Plan, and the District land use standards. This Plan also provides the administrative procedures and requirements for the implementation of the Plan.



## PROCESS

The First Ward Redevelopment Area was originally designated as an “an area in need of redevelopment” in May of 2003, along with areas in the Fourth and Fifth Wards of the City. These redevelopment area designations came as a result of the City’s 2003 Master Plan, which recommended that many of the City’s central residential neighborhoods be investigated for their potential to be designated as “areas in need on redevelopment” to help catalyze revitalization. This resulted in the Planning Board undertaking formal redevelopment studies to examine each of these areas, and it was determined at that time that these areas were statutorily in need of redevelopment.

After the redevelopment designation, a formal Redevelopment Plan for the First Ward was adopted by the City Council in October of 2003, which is the document governing land use within the Redevelopment Area.

In April of 2014, the City’s Planning Board adopted a new Master Plan document. As a part of the process of developing the Master Plan, public meetings were held in each Ward of the City, to discuss issues specific to those areas.

The following key issues were noted regarding the First Ward Redevelopment Area:

- Haledon Ave is a gateway to the City, and should be treated as such.
- 1st ward needs a new library. It was destroyed by flooding and other libraries do not serve 1st Ward residents.
- A community resource center housing

computers and adult education classes would be very welcome, and could replace the library and its previous functions.

- Abandoned properties are troublesome throughout the neighborhood.
- There is a need to plan for future flood events.

Together North Jersey, a collaborative planning initiative has sponsored two recent studies in the First Ward Redevelopment Area. A Crime Prevention Through Environmental Design audit was completed in April 2015, and the Northside Community Action Plan was completed in February 2015.

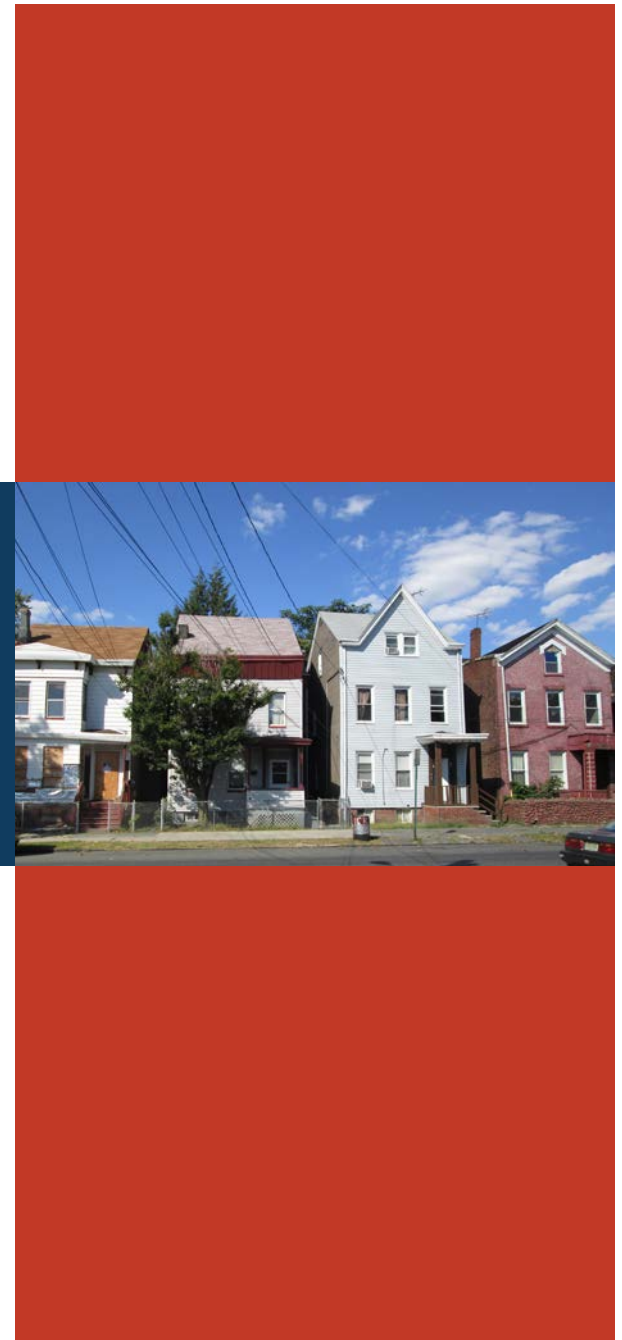
This update to the First Ward Redevelopment Plan is a direct output of the 2014 Master Planning process and seeks to build on the other planning projects that have been prepared for the Area.



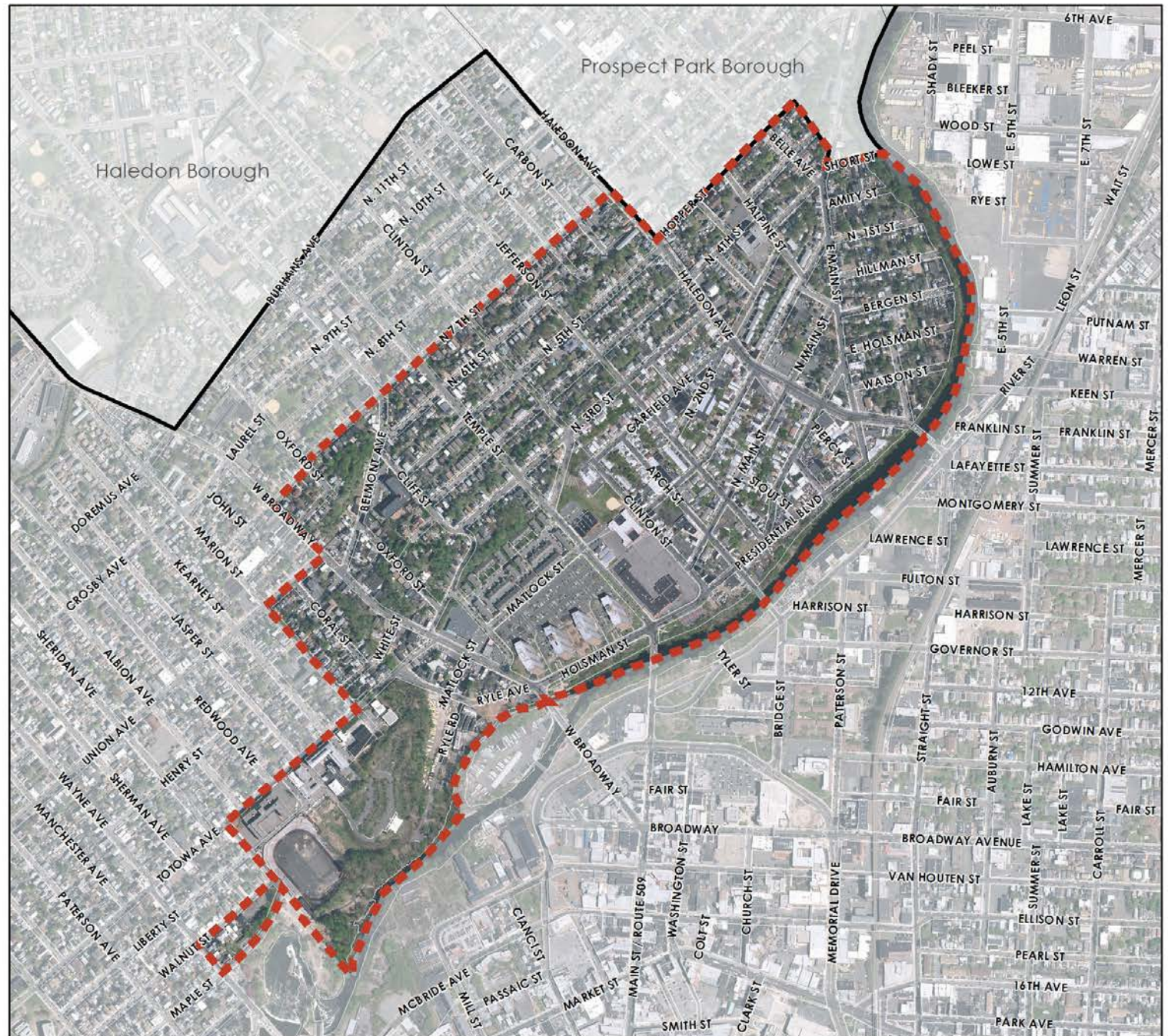


# PLANNING CONTEXT

## PATERSON FIRST WARD









## DESCRIPTION OF AREA

### Boundary Description

The First Ward Redevelopment Area (the Area) is over 200 acres of land located on the northern and western sides of the Passaic River. The boundaries of the area run along Wayne Avenue, Totowa Avenue, Marion Street, Garrison Street, N.7th Street, Haledon Avenue, Hopper Street and the Passaic River. The Area borders the Borough of Prospect Park to the north along Haledon Avenue, Hopper Street, and East Main Street. The Great Falls National Historical Park lies just southwest of the Redevelopment Area, and portions of the Redevelopment Area are within the National Park boundaries.

### Layout and Orientation

The general layout of the area is that of a traditional urban street grid which follows the contours of the Passaic River. Throughout much of the area, the streets are either perpendicular or parallel to the River, forming typical rectilinear city blocks. These blocks become irregularly shaped in the northern portions of the Area where the River curves and streets merge at odd angles, leaving triangular shaped blocks along several streets. The southern portions of the Area along the river, west of West Broadway differ from the rest of the Area in that there is no typical street grid pattern creating typical blocks, but only very large parcels of land between Totowa Avenue and the Passaic River. Most of the streets in this section of the Area are dead end streets.

Although much of the Area layout is along a traditional street grid, the streets and blocks are

not flat and uniform. There is a steep change in grade just northwest of the Passaic River along many of the streets that run perpendicular to the River. There may be a height difference of as much as 40 to 50 feet from one end of a block to another. There are cliffs and hills throughout the Area.

The primary exception to this hilly terrain is the blocks east of East Main Street in the northeastern most sections of the Area. These blocks are relatively flat and low lying. This, along with other factors, has left them prone to flooding during heavy rains and storms, as they are adjacent to the Passaic River.



*Residences in the Area*



*Steep Terrain*



*Presidential Boulevard*

## Land Use

### Land Use in the Area

Of the 200+ acres within the Area, over 40% of the developable land is devoted to residential uses, either single or two family homes. This figure increases to nearly 50% when apartment buildings are included. These residential uses make up most of the northern and western sections of the Area, where most of the rectilinear blocks are split into typical 2,000 – 3,500 square foot lots developed with 2 story homes. This has been the case in the Area since the original adoption of a Redevelopment Plan for the Area, as the area of residential properties has changed by less than 1 acre during the past 12 years according to the City's tax records.

Mixed amongst the residences are a number of small scale commercial structures, many of which also have residences located above or behind a retail store. Both Haledon Avenue and West Broadway can be considered commercial/retail corridors that serve their respective neighborhoods, and offer a number of options for local residents. However, much of what is considered commercial property for tax purposes as identified in the chart below, is not traditional retail, office space, or a service business, but is more industrial related commercial uses such as auto-repair or junk yards and scrap yards. There are a number of these types of businesses located along the River throughout the Area. The percentage of commercial properties in the Area has increased during the past 12 years, indicating that the number of small businesses may have increased.

The percentage of industrial lands in the Area has slowly increased throughout the past decade. Most of the industrial properties are located in the southwestern end of the Area near the Great Falls and along Totowa Avenue. These industrial uses are primarily larger factories that have been in the Area for many decades, dating back to the City's peak manufacturing periods in the early and mid-20th Century.

Outside of residential uses, the most prevalent land use in the Area is public uses, either schools or other public property. Over 15% of the Area is devoted to uses by public schools, parks, and other government facilities. The number of public properties has decreased by over 25% during the

past 12 years. This may be due to property that was previously municipally owned, being sold or donated to private community based organizations, as many properties that were previously listed as being public property, are now listed as private property, but remain tax exempt.

While the amount of properties that are classified as vacant has been reduced since 2004, the number of vacant properties continues to be an issue in the First Ward Area. Almost 9% of properties are classified as vacant, meaning they are undeveloped. This does not include the number of properties that are developed with a structure, but have been abandoned, which is also a key issue in the Area.

First Ward Redevelopment Area Land Use: 2004-2016				
Property Classification	2016		2004	
	Acres	Percentage	Acres	Percentage
Vacant	19.3	8.8%	20.1	9.8%
Residential	89.9	41.2%	91.1	44.4%
Apartment	15.0	6.9%	3.7	1.8%
Commercial	23.4	10.7%	19.9	9.7%
Industrial	11.5	5.3%	9.9	4.8%
Railroad	0.0	0.0%	0.0	0.0%
Public School	18.4	8.4%	18.4	9.0%
Private School	0.0	0.0%	0.0	0.0%
Public Property	24.6	11.3%	21.0	10.2%
Church & Charitable	7.7	3.5%	7.2	3.5%
Other Tax Exempt	8.4	3.8%	12.3	6.0%
Unknown	0.0	0.0%	1.8	0.9%

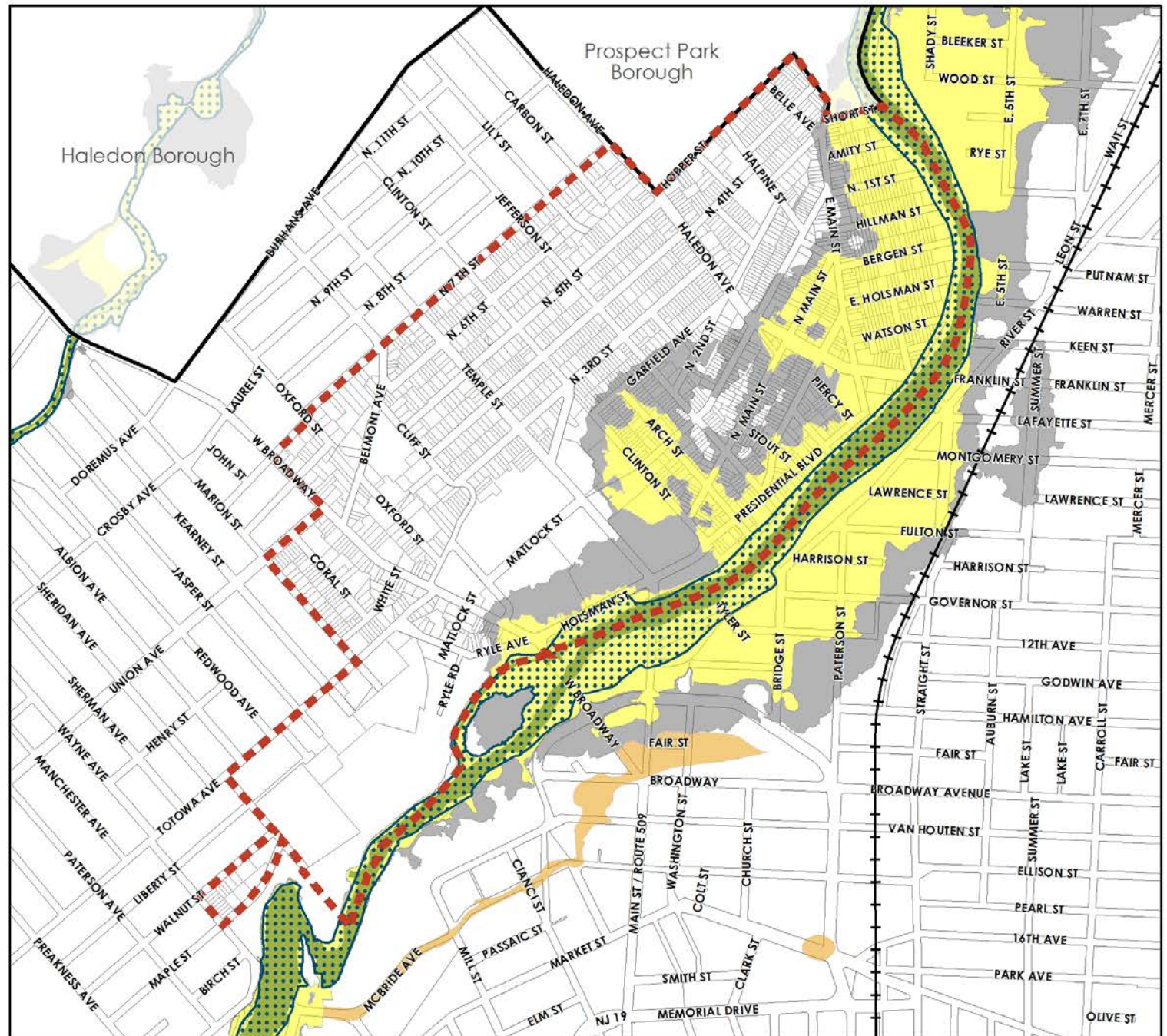
Source: MOD IV Tax Records







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### ***Building Conditions***

When the Area was designated as an “area in need of redevelopment” in 2003, it was due to a combination of having buildings that were substandard or dilapidated, abandoned, or obsolete for a number of reasons. Throughout much of the Area, the conditions that led to a redevelopment designation remain. While some buildings and properties have been maintained and renovated, a number of other buildings either remain or have fallen into a state of disrepair. Although these conditions are present throughout the Area, their impacts are most concentrated in the neighborhoods on the eastside of the Area.

### ***Flood Hazard Areas***

According to data from the Federal Emergency Management Administration (FEMA), many sections of the First Ward Redevelopment Area fall within either the floodway, or within the ‘100 year floodplain’, which means that there is a 1% chance in any given year that the area will be inundated with flood waters from a storm. Unfortunately for the areas shown in the FEMA Floodplain Map, this has been the case several times in recent years. The areas east of East Main Street, and along Arch Street have experienced significant flooding during the storm events of Hurricane Irene and other major storms where the waters of the Passaic River have flooded.



*Abandoned Building*



*Abandoned Building*



*Vacant and Abandoned Property*



*Poor Building Condition*



*Abandoned Building after Flood Damage*



*Fire Damaged Building*



School Number 28



Public School Recreation Field



School Number 14

## COMMUNITY FACILITIES

### Community Resources in the Area

There are several public schools that are located within the First Ward Redevelopment Area. All of these schools are either elementary or intermediate level schools. The City's high schools are located in other neighborhoods outside of the Area.

Schools in the Area:

- Public School #5 – 430 Totowa Avenue (grades K-8)
- Public School #28 – 200 Presidential Boulevard (grades K-8)
- Napier Academy – 55 Clinton Street (grades 5-8)
- Urban Leadership Academy – 112 North Fifth Street (grades 1-5)
- Public School #14 – 522 Union Avenue (grades K-4)
- Public School #12 – 121 North Second Street (grades K-8)

In addition to the public schools serving the Area, there are several other community facilities that provide services to residents. These facilities include:

- Christopher Hope Center (includes temporary home of Northside Branch Library) – 60 Temple Street
- Paterson Fire Department, Northside Firehouse – Temple Street
- Hinchliffe Stadium – Maple Street/Liberty Street

There are also a number of non-profit, religious, and faith based organizations and houses of worship located throughout the Area, which provide services to residents.

### Parks and Open Space

The largest park within and near the First Ward Redevelopment Area is the Great Falls National Historical Park. The National Park includes Hinchliffe Stadium, and the Valley of the Rocks, which are within the Redevelopment Area, as well as Mary Ellen Kramer Park, and Overlook Park, just outside the area, both of which provide views of the Great Falls themselves.

There are two other park spaces within the Area, Cobb Park, a playground at the corner of East Main Street and North Main Street, and the Riverfront park along the Passaic River between West Broadway and Clinton Street. The Riverfront park includes general green space in the form of lawns, as well benches, tables, and several basketball courts. There is playground equipment and benches within the Cobb Park community playground on East Main Street.

There is additional recreation space at the schools within the Area, as the parking lots for these schools are often used as basketball courts or other play spaces by the students who attend them.

In addition to the established parks, the City of Paterson has recently received funding through the State of New Jersey Green Acres program to acquire the "Vistas" site and several properties along Ryle Avenue. These properties will complement the Great Falls National Historical Park.



## Parks, Open Space & Educational Facilities

-  Parks/Open Space
  - Educational Facilities**
    -  Passaic County Community College
    -  Private School
    -  Public School
    -  Charter School
    -  Preschool
  -  First Ward Redevelopment Area



Source: NJGIN, NJDEP, NJOGIS, NJDOT,  
City of Paterson, 2014 MOD IV Data

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First Ward Redevelopment Plan

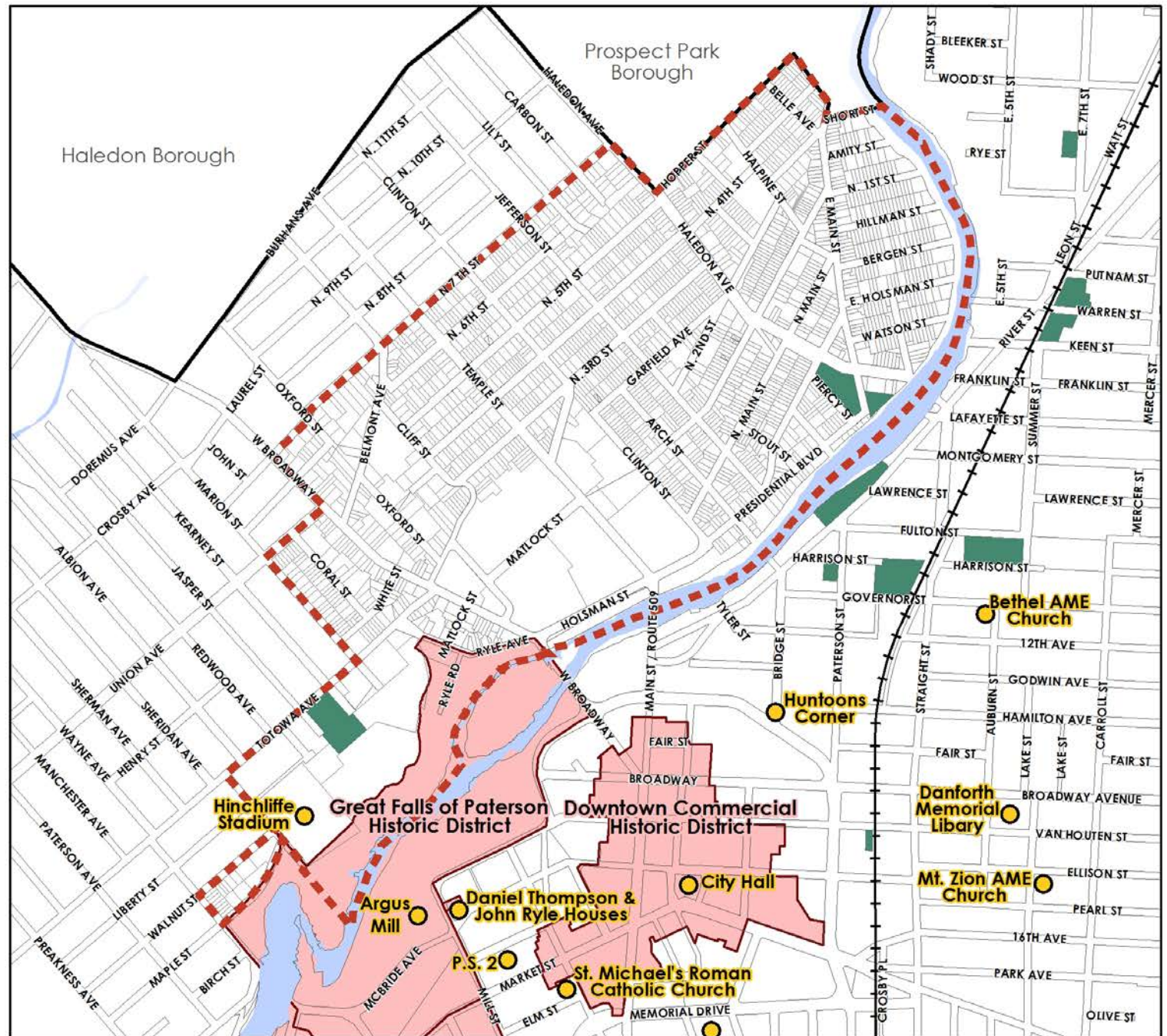
## Historic Places & Districts

-  Historic Places
-  Historic Mill Survey
-  Historic Districts
-  First Ward Redevelopment Area



Source: NJGIN, NJDEP, NJOGIS, NJDOT,  
City of Paterson

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### Historic Resources

There are several identified historic resources within the First Ward Redevelopment Area.

- **Hinchliffe Stadium** – This historic stadium which is situated just north of the Great Falls of the Passaic River has recently been designated as a National Historic Landmark structure. The City has begun efforts to revitalize the stadium including façade stabilization and the anticipated construction of new amenities on the site that will complement both the stadium and the Great Falls National Historical Park. The stadium was originally constructed in 1932, and was the home of the New York Black Yankees, a baseball team that played in the Negro Leagues. The stadium was also used for football and soccer games, as well as auto racing among other things.
- **Miesch Silk Manufacturing Company (Totowa Works)** – This silk manufacturing plant just a block east of Hinchliffe Stadium is a series of three red brick industrial buildings that date back to the early 20th Century. This site is listed on the City's 2012 Historic Mill Survey, and is eligible for designation on the National Register of Historic Places.
- **National Silk Dyeing Company (East Main Street Works)** – The mill structures on the corner of East Main Street, Haledon Avenue, and Percy Street date back to 1910, and were used as a silk dyeing and finishing mill. This complex of small industrial buildings is listed in the City's 2012 Historic Mill Survey, and is also noted as being eligible for Federal Historic Preservation designation.



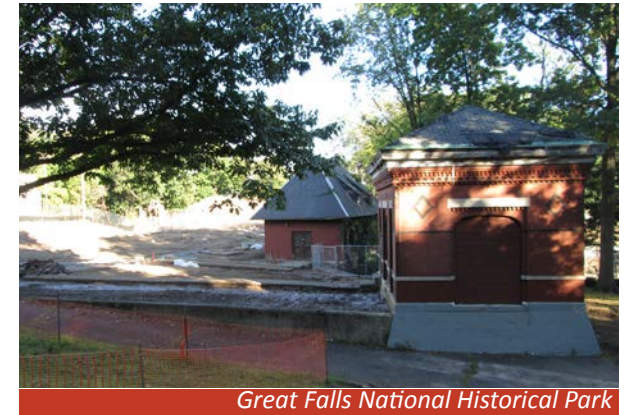
*National Silk Dyeing Company*



*Miesch Silk Manufacturing*



*Hinchliffe Stadium*



*Great Falls National Historical Park*



*Great Falls Bike Trail*



*Great Falls of Paterson*

- Great Falls National Historic District – The Great Falls National Historical Park and the federally designated historic district are partially within the southwest portion of the Area. The Great Falls themselves and the surrounding historic district is the oldest and most important area in the development of the City, and the development of the industrial revolution in the United States.

### URBAN ENTERPRISE ZONE

The Urban Enterprise Zone is a state program designed to help encourage employment and economic development in urban areas through incentives such as a reduced sales tax for consumers, and subsidized unemployment insurance for employers. In designated Urban Enterprise Zones, sales tax is reduced to 3.4375% rather than 6.875%. UEZ member businesses also receive tax exemption on many operating expenses incurred. The UEZ also helps to administer and coordinate improvements to business areas as well as assist with small business loans to members. Within the First Ward Redevelopment Area, the majority of the areas along Haledon Avenue, West Broadway, Belmont Avenue, and portions of East Main Street are within the designated UEZ boundaries of the City, as shown in the Urban Enterprise Zone Map.

### TRANSPORTATION



*Business in the First Ward*



*Business on Belmont Avenue*



*Business in the First Ward*



City of Paterson, NJ

First Ward Redevelopment Plan

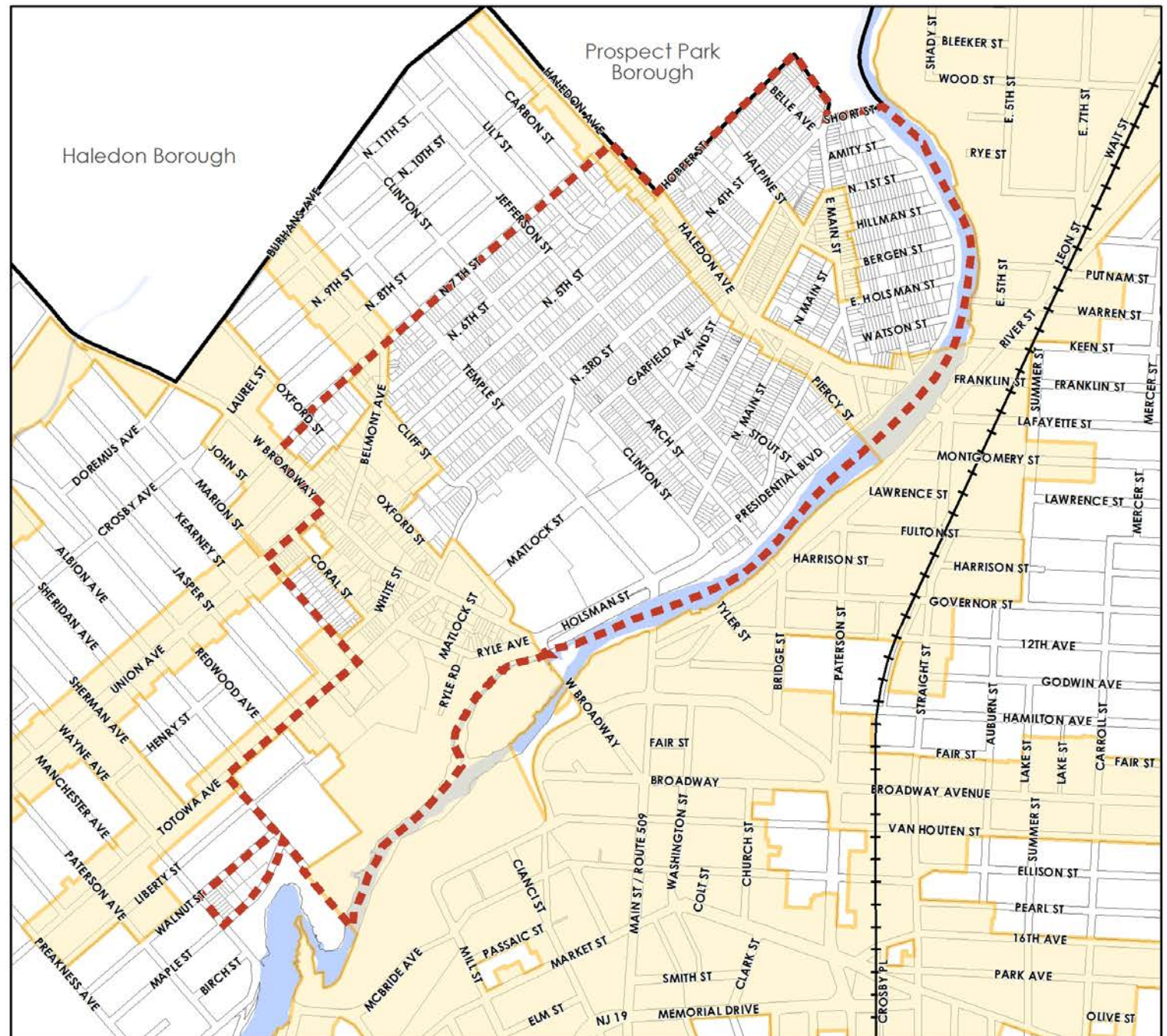
## Urban Enterprise Zone

Urban Enterprise Zone  
First Ward Redevelopment Area



Source: NJGIN, NJDEP, NJOGIS, NJDOT,  
City of Paterson

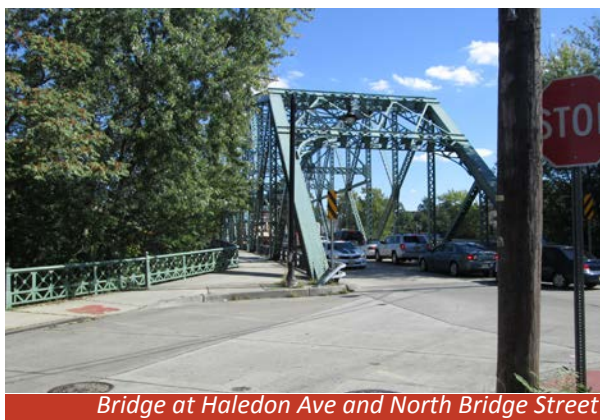
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*Nj Transit Bus Service in Paterson*



*Bridge at Haledon Ave and North Bridge Street*



*Presidential Boulevard*

### ***Circulation in the Area***

The primary arterial streets in the Area are:

- West Broadway (Passaic County Route 675)
- Haledon Avenue (Passaic County Route 504)
- East Main Street (Passaic County Route 504)
- Presidential Blvd.(Passaic County Route 509)
- Union Avenue (Passaic County Route 646)

These five streets provide the connections to the greater City and surrounding areas to and from the First Ward Area, while most of the other streets in the Area serve as local roads. West Broadway and Haledon Avenue serve as north/south connectors, connecting Downtown and the center of Paterson to areas northwest of the City. Presidential Boulevard runs along the Passaic River, connecting the east and west sides of the First Ward. Union Avenue is a major commercial corridor in the City, although the majority of the commercial businesses on the street are located further southwest, outside the boundaries of the First Ward Redevelopment Area. East Main Street connects the Straight Street bridge and Haledon Avenue to areas north of the City in Prospect Park.

### ***Public Transportation in the Area***

There are three NJ Transit bus routes that provide service within the First Ward Redevelopment Area.

- Route 703 – This route runs between Haledon and East Rutherford, making stops in Carlstadt, Moonachie, Passaic City, and Paterson. This route runs regularly throughout the week approximately every 30 minutes, and on weekends, making

several stops along Belmont Avenue and West Broadway in between Haledon and the Meadowlands Sports Complex, which is its terminal destination. This route also connects to Downtown Paterson, making stops at the Broadway Bus Terminal, and other points.

- Route 744 – This route runs between Wayne and Passaic City, making stops in Haledon, Prospect Park, Paterson, and Clifton. The route runs from the Preakness Shopping Center in Wayne, to the Main Avenue Terminal in Passaic. This route makes stops within the First Ward Redevelopment Area along Haledon Avenue and Presidential Boulevard. This route runs approximately every 30 minutes on weekdays and Saturdays from 6:00 am until 9:00 pm, and connects with Downtown Paterson and multiple points of interest in Wayne and Clifton as well.
- Route 748 – This route runs between Wayne and Paterson, making stops in Paterson, Wayne, and Pompton Lakes. Local service in the First Ward Redevelopment Area is provided along West Broadway. This route runs regularly once an hour on weekdays and Saturdays only.




From Downtown Paterson, either at the Broadway Bus Terminal or at the Ward Street Train Station, there are many other connections throughout the region that can be accessed. NJ Transit Paterson Ward Street train station is approximately 3/4 of a mile from the First Ward Redevelopment Area, and the Broadway Bus Terminal is just south of the Area.


City of Paterson, NJ

First Ward Redevelopment Plan

Street Network

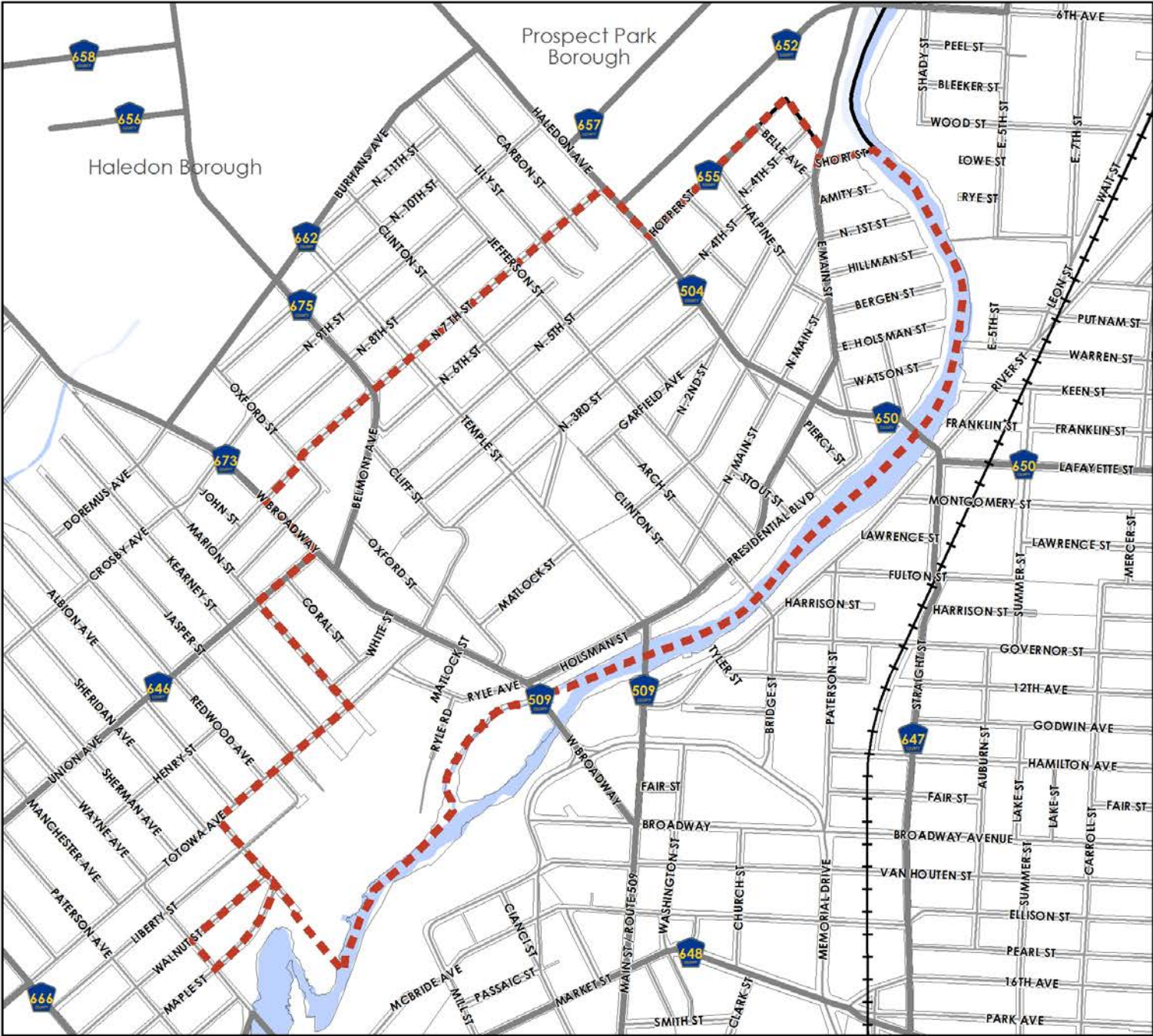
**Road Classification**

-  County Route
-  Local Roads
-  First Ward Redevelopment Area



Source: NJGIN, NJDEP, NJGIS, NJDOT,  
City of Paterson

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City of Paterson, NJ

First Ward Redevelopment Plan

Public Transportation

 Train Station

**Bus Stops**

-  703
-  744
-  748
-  Other Bus Stops

**Bus Routes**

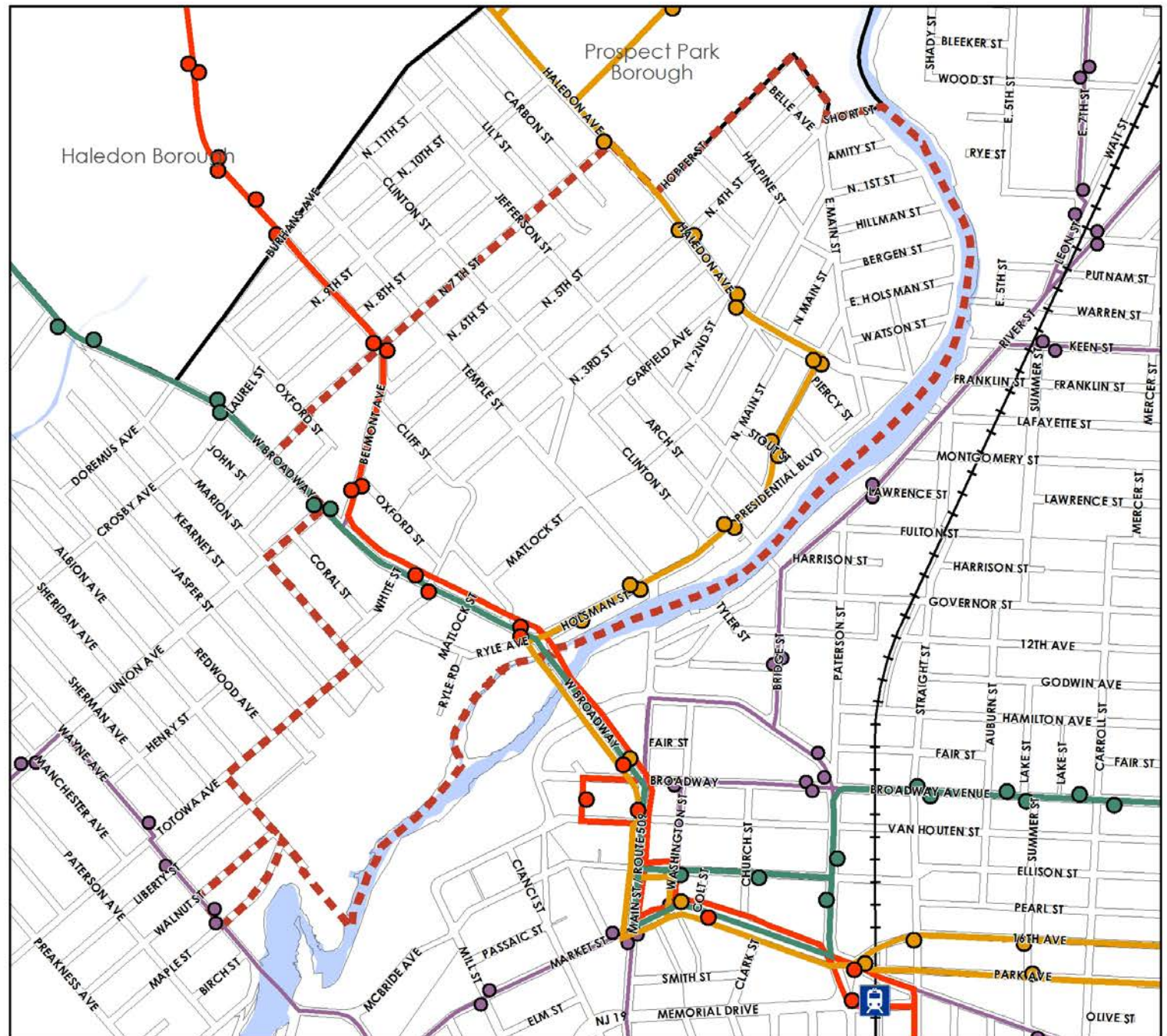
-  703
-  744
-  748
-  Other Bus Routes

 First Ward Redevelopment Area

 0 500 1,000 Feet

Source: NJGIN, NJDEP, NJOGIS, NJDOT,  
City of Paterson

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# GOALS & OBJECTIVES

## PATERSON FIRST WARD





## GOALS AND OBJECTIVES

- **Revitalize the First Ward of the City**

The primary goal of this Plan, and the redevelopment effort is to bring economic and community development to the Area, and to improve the quality of life for residents and businesses in the Area.

- **Assemble parcels for larger development projects**

While the redevelopment of the First Ward Redevelopment Area will be mostly infill development, identifying larger properties or tracts of contiguous smaller properties could provide opportunities for more substantial redevelopment projects that could have a greater impact on the Area.

- **Reduce the blighting impacts of abandoned/vacant properties**

The First Ward Redevelopment Area has been impacted tremendously by properties becoming abandoned or left vacant, more so than most other areas of the City. Addressing dilapidated and abandoned buildings can transform the Area.

- **Provide quality new housing options for area residents**

Much of the housing in the Area is in poor condition. New housing that caters to a range of potential residents can bring new life to the Area.

- **Create cohesive, and vibrant commercial districts to serve the local population**

Area revitalization efforts must include commercial businesses to serve the

residential population. Revitalized neighborhood commercial districts can improve quality of life for local residents, as well as potentially bring in outside investment to the community. Land uses that have been identified as magnets for criminal activity and are perceived as detrimental to quality of life are discouraged.

- **Promote mixed use development**

The City should encourage traditional forms of housing and retail, where store owners can have the option of living above their store. Allowing mixed uses in the commercial corridors of the Area can help develop small businesses, as well as provide the mixing of uses and people that help commercial areas succeed.

- **Reduce the impacts of flooding from storm events**

This Area of the City has been heavily impacted by flooding. Many residences and businesses are located along the banks of the Passaic River and under the threat of further damage during any heavy storm. When resources are continually devoted to rebuilding or cleaning up after a storm just to return to the status quo, this reduces the amount of resources that can be used for other more productive purposes.

- **Establish a greenbelt along the Passaic River**

A series of parks and open space alongside the banks of the Passaic River can provide needed recreation space to residents, as well as help to mitigate the impacts of flooding.

- **Facilitate connections to the National Park**

The western end of the Area surrounds the Great Falls National Historical Park, and should function as a gateway to the park for visitors

- **Promote the restoration of Hinchliffe Stadium**

Restoring Hinchliffe Stadium and integrating it and Mary Ellen Kramer Park with new amenities and attractions as part of the northern anchor of the Great Falls National Historical Park will attract visitors and spur economic activity in the First Ward. These historical features have the potential to drive the economic revitalization of the City and the neighborhood.

- **Provide additional community facilities**

The First Ward Branch of the Paterson Free Public Library was flooded during Hurricane Irene and has not re-opened. Restoration of the Library building and establishment of additional community resource and meeting spaces, community parks, gardens, and recreational activities should be pursued.

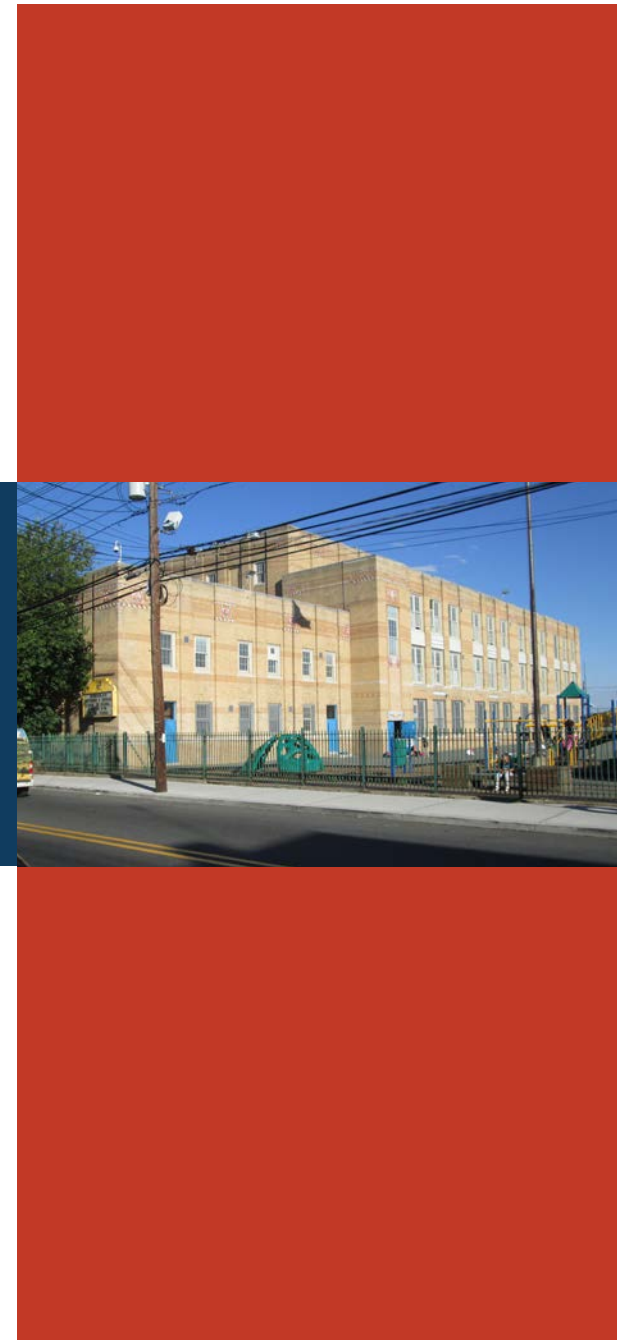
- **Enhance public safety**

The First Ward Redevelopment Area is continuing to deal with high rates of crime. The plan encourages improved code enforcement to address building and property maintenance issues, additional police and department of public works presence to discourage illegal activity and clean-up the neighborhood, and incorporation of Crime Prevention Through Environmental Design (CPTED) principals to enhance the built environment.



# DISTRICT STANDARDS

## PATERSON FIRST WARD





### **RELATIONSHIP TO THE CITY'S LAND DEVELOPMENT REGULATIONS**

The Area shall be redeveloped in accordance with the standards detailed in this Redevelopment Plan. The Plan supersedes the use, bulk, and design standard provisions of the City's Land Development Regulations. Unless otherwise specified within this Plan, all definitions from the City's Land Development Ordinance shall apply.

The Board of Adjustment shall have the authority to grant relief from standards that require a "d" variance pursuant to N.J.S.A. 40:55D-70d. The Planning Board shall have power to grant relief from other bulk and dimensional requirements of this Plan to the same extent as the Board may grant relief from bulk and dimensional requirements pursuant to N.J.S.A. 40:55D-70c.

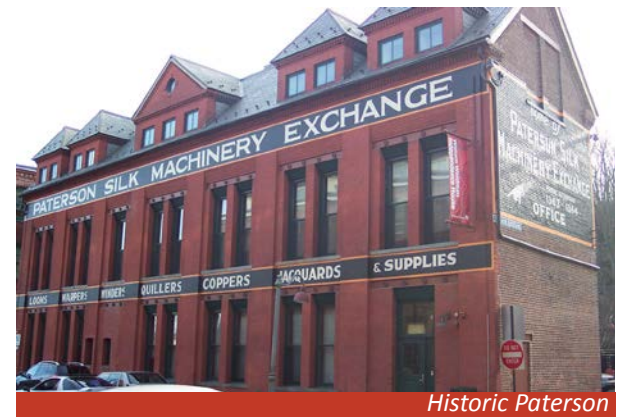
All exceptions or waivers from design standards from the requirements for site plan or subdivision approval may be granted by the City Planning Board or Zoning Board of Adjustment.

All development must be approved by the appropriate Board and shall be submitted through the normal site plan and subdivision procedures as identified by N.J.S.A. 40:55D-1, et seq.

Final adoption of this Redevelopment Plan by the City Council shall be considered an amendment to the City Zoning Ordinance and Official Zoning Map.



*Paterson City Hall*



*Historic Paterson*



City of Paterson, NJ

First Ward Redevelopment Plan

## Land Use Districts

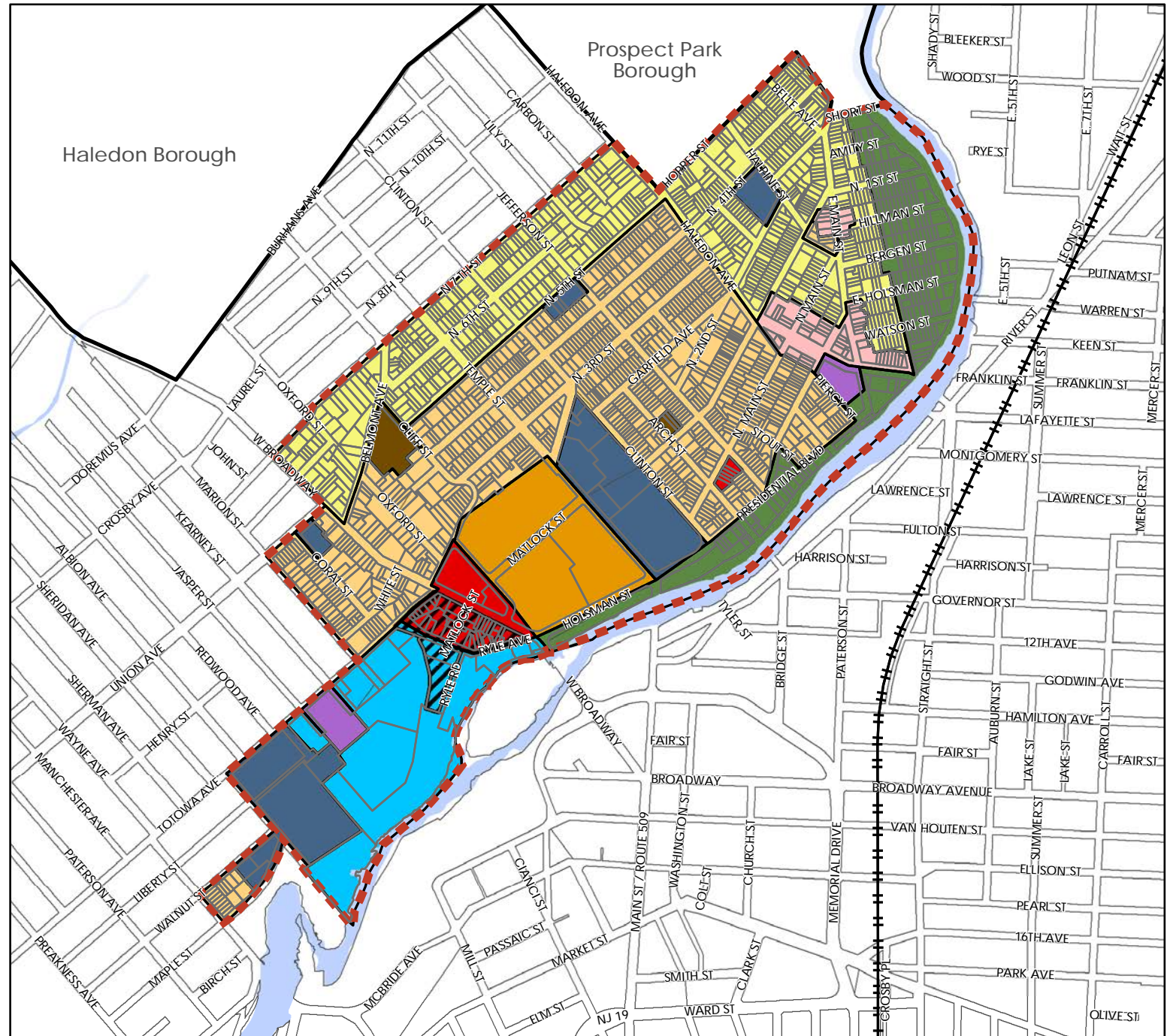
### Proposed Zoning

- RA-1: Residential
- RA-2: Residential
- R-4 Residential
- SR: Senior Residential
- C-1: Neighborhood Commercial
- C-2: General Commercial
- PU: Public Use District
- AR Adaptive Reuse District
- OS: Open Space
- NPGD: National Park Gateway District
- LW: Live/Work Overlay District
- First Ward Redevelopment Area



Source: NJGIN, NJDEP, NJOGIS, NJDOT,  
City of Paterson

HEYER GRUEL & ASSOCIATES





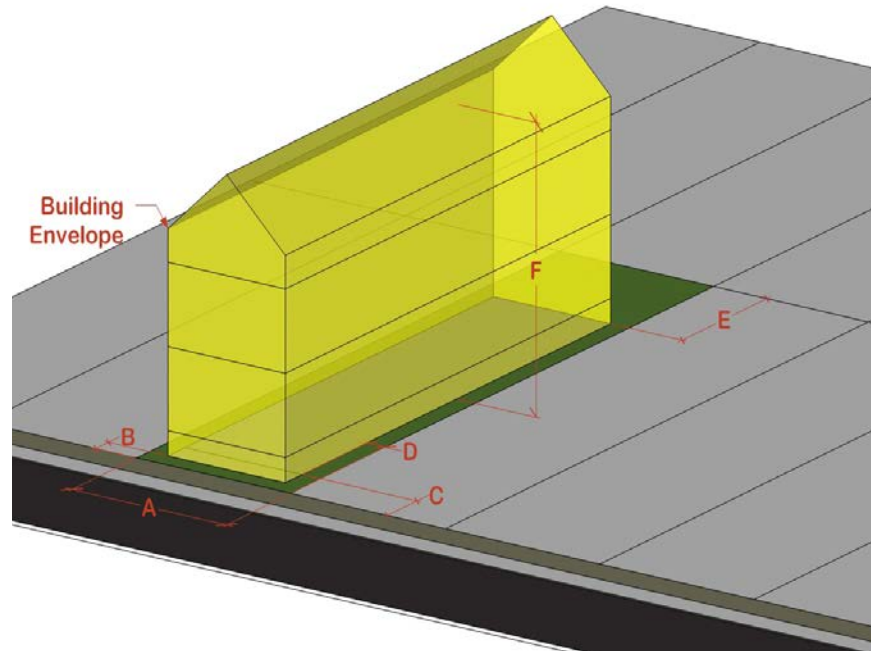
**RA-1 RESIDENTIAL DISTRICT****Intent**

The intent of the RA-1 residential district is to permit single and two family dwellings. It is the further intent of this district to maintain the residential character of the neighborhood. In keeping with this intent, multiple family dwellings and stand-alone commercial uses are prohibited.

**Principal Permitted Uses**

- Detached single-family dwellings.
- Two-family dwellings.
- Community gardens.
- Public facilities.
- Mixed-use development which includes residential and use permitted in the C-1 Neighborhood Commercial district under the following conditions:
  - Maximum lot width: fifty (50) feet.
  - These uses shall only be located at the corners of blocks. No mid-block mixed use development is permitted.
  - Neighborhood commercial uses shall only be permitted on the first floor of any building. At least one floor of residence(s) is required above the commercial use.
  - No off-street parking shall be required for a commercial use.
- Places of worship subject to the conditions in Chapter 500 Section 500-5.3 of the City's Land Development Ordinance.

RA-1 Single/Two Family Bulk Standards Diagram



Single/Two Family Dwelling	
A	Minimum Lot Width
B	Minimum Front Yard Setback
C	Maximum Front Yard Setback
D	Minimum Side Yard Setback
E	Minimum Rear Yard Setback
F	Maximum Building Height

RA-1 Single/Two Family Bulk Standards Diagram



Single Family Housing

**Accessory Uses**

- Private garages.
- Home occupations.
- Any use customarily incidental and accessory to the principal use.

**Parking**

Lots with a width of fifty (50) feet or greater prior to subdivision and/or development shall provide off-street parking. No off-street parking is required on existing lots that are less than fifty (50) feet in width.

- Off-street parking shall be accessed by a driveway not wider than twelve (12) feet.
- The following off-street parking shall be required.
  - One (1) space per unit for dwellings with one (1) bedroom.
  - One and one-half (1.5) spaces per unit for dwellings with two (2) bedrooms.
  - Two (2) spaces per unit for dwellings with three (3) or more bedrooms.
  - One (1) space per unit for residences in a mixed use development.

**Area, Yard and Building Requirements**

See RA-1 Residential District's Bulk Standards Chart

RA-1 District Bulk Standards	
Minimum Lot Area	None
Minimum Lot Width	Twenty-five (25) feet <sup>1</sup>
Minimum Front Yard	Three (3) feet <sup>2</sup>
Maximum Front Yard	Seven (7) feet
Minimum Side Yard	Three (3) feet
Minimum Rear Yard	Twenty (20) feet
Maximum Building Height	Thirty-five (35) feet and two and a half (2.5) stories
Maximum Building Coverage	Sixty percent (60%)
Maximum Impervious Surface Coverage	Eighty percent (80%)
1. This applies only to newly created lots. Lots existing at the time of adoption of this Plan are exempt from minimum lot width standards and may be grandfathered in.  2. Or the average setback within 200 feet on each side of the lot, not to exceed Seven (7) feet.	

## RA-2 RESIDENTIAL DISTRICT

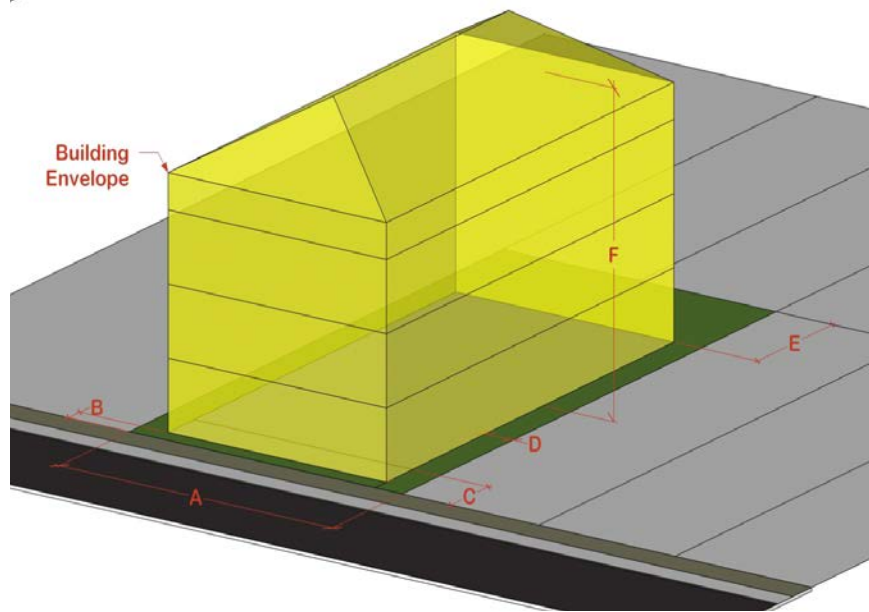
### Intent

The RA-2 residential district is designed to permit a more intensive residential use of land with various types of dwellings. Density is maintained in medium range, while building height is kept low enough to be generally compatible with one and two-family residential development.

### Principal Permitted Uses

- Detached single-family dwellings.
- Attached single-family dwellings, townhouses
  - Maximum of four (4) units per building.
- Two-family dwellings.
- Three- to four-family dwellings.
- Community gardens.
- Public facilities
- Mixed-use development which includes residential and use permitted in the C-1 Neighborhood Commercial district under the following conditions:
  - Maximum lot width: fifty (50) feet.
  - These uses shall only be located at the corners of blocks. No mid-block mixed use development is permitted.
  - Neighborhood commercial uses shall only be permitted on the first floor of any building. At least one floor of residence(s) is required above the commercial use.
  - No off-street parking shall be required for a commercial use.
- Places of worship subject to the conditions in Chapter 500 Section 500-5.3 of the City's Land Development Ordinance.

RA-2 Three/Four Family Bulk Standards Diagram



Three/Four Family Dwelling

A	Minimum Lot Width
B	Minimum Front Yard Setback
C	Maximum Front Yard Setback
D	Minimum Side Yard Setback
E	Minimum Rear Yard Setback
F	Maximum Building Height

RA-2 Three/Four Family Bulk Standards Diagram



RA-2 Single/Two Family Bulk Standards Diagram





**Accessory Uses**

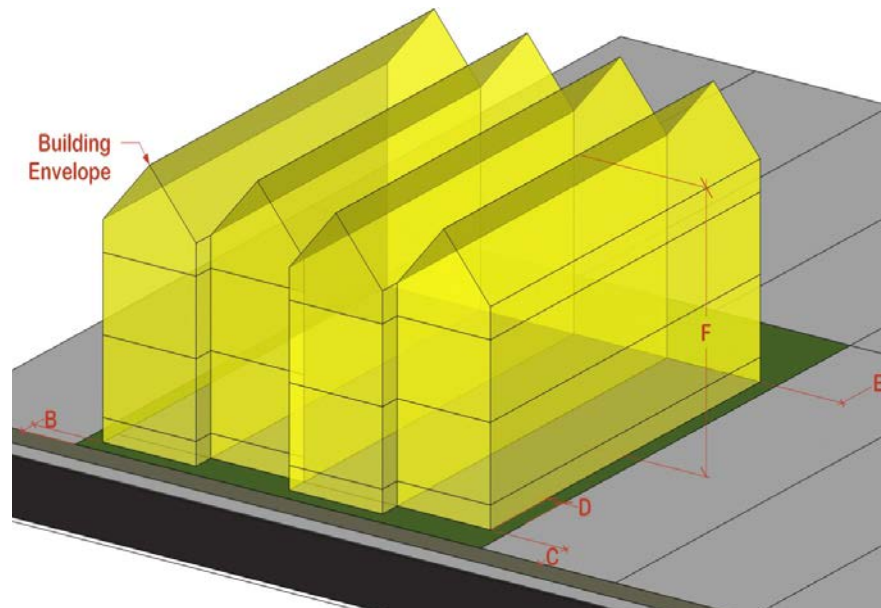
- Private garages
- Private greenhouses and any agricultural or horticultural activities not for business purposes, incidental to residential use; provided that any storage of fertilizer shall be distant at least fifty (50) feet from a property line
- Home occupations.
- Any use customarily incidental or accessory to the principal use.

**Parking**

Lots with a width of fifty (50) feet or greater prior to subdivision and/or development shall provide off-street parking. No off-street parking is required on existing lots that are less than fifty (50) feet in width.

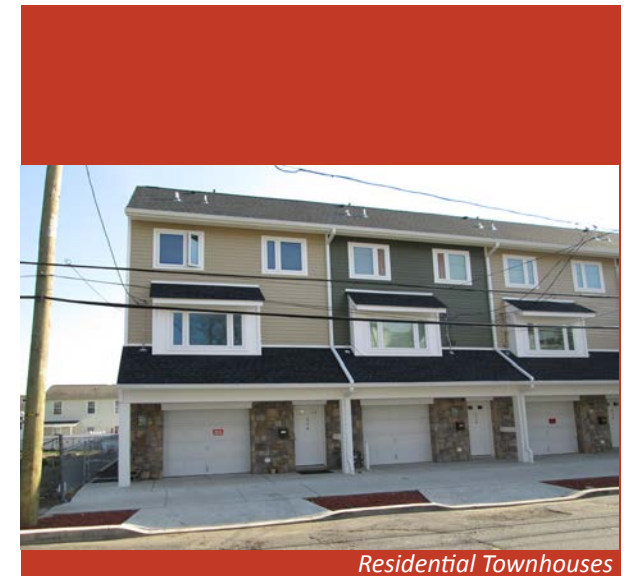
- Off-street parking shall be accessed by a driveway not wider than twelve (12) feet.
- The following off-street parking shall be required.
  - One (1) space per unit for dwellings with one (1) bedroom.
  - One and one-half (1.5) spaces per unit for dwellings with two (2) bedrooms.
  - Two (2) spaces per unit for dwellings with three (3) or more bedrooms.
  - One (1) space per unit for residences in a mixed use development.

RA-2 Townhouse Bulk Standards Diagram

**Townhouse Dwellings**

A	Minimum Lot Width	
B	Minimum Front Yard Setback	
C	Maximum Front Yard Setback	
D	Minimum Side Yard Setback	
E	Minimum Rear Yard Setback	
F	Maximum Building Height	Building Height

RA-2 Townhouse Bulk Standards Diagram

*Residential Townhouses*

### Usable Open Space or Amenity Space

All multiple family dwellings in the RA-2 district shall provide a minimum of 150 square feet of exterior open space or interior amenity space per dwelling unit. These spaces may include, but are not limited to, landscaped outdoor areas with seating, spaces for gardening, rooftop terraces, balconies, fitness centers, and meeting rooms.

### Area, Yard and Building Requirements

See RA-2 Residential District's Bulk Standards Chart

RA-2 District Bulk Standards	
<i>Single Family or Two-Family Dwellings</i>	
Minimum Lot Area	None
Minimum Lot Width	Twenty-five (25) feet <sup>1</sup>
Minimum Front Yard	Three (3) feet <sup>2</sup>
Maximum Front Yard	Seven (7) feet
Minimum Side Yard <sup>3</sup>	Three (3) feet
Minimum Rear Yard	Twenty (20) feet
Maximum Building Height	Thirty-five (35) feet and two and a half (2.5) stories
Maximum Building Coverage	Sixty percent (60%)
Maximum Impervious Surface Coverage	Eighty percent (80%)
<p>1. This applies only to newly created lots. Lots existing at the time of adoption of this Plan are exempt from minimum lot width standards and may be grandfathered in.</p> <p>2. Or the average setback within 200 feet on each side of the lot, not to exceed Seven (7) feet.</p> <p>3. For attached dwellings (townhouses) side yard setbacks shall apply to the end of the building, and not individual units.</p>	

RA-2 District Bulk Standards	
<i>Three-Family or Four-Family dwellings</i>	
Minimum Lot Area	5,000 square feet
Minimum Lot Width	Fifty (50) feet
Minimum Front Yard	Three (3) feet <sup>1</sup>
Maximum Front Yard	Ten (10) feet
Minimum Side Yard	Five (5) feet
Minimum Rear Yard	Twenty (20) feet
Maximum Building Height	Forty-five (45) feet and Three and a half (3.5) stories
Maximum # Dwelling Units/Acre	Fifty-five (55)
Maximum Building Coverage	Sixty percent (60%)
Maximum Impervious Surface Coverage	Eighty percent (80%)
<p>1. Or the average setback within 200 feet on each side of the lot, not to exceed Seven (7) feet.</p>	

**R-4 HIGH DENSITY RESIDENTIAL DISTRICT****Intent**

The R-4 High Density Residential District is designed to permit high-rise apartment buildings to accommodate high density housing needs in areas with good transportation access. Also permitted in this district are other residential uses.

**Principal Permitted Uses**

- Detached single-family dwellings.
- Detached two-family dwellings.
- Attached single-family dwellings.
- Multi-family residences.
- Garden apartments.
- Low-Rise apartments.
- High-Rise apartments.

**Accessory Uses**

- Off-street parking.
- Swimming pools.
- Family child day care homes
- Home Offices
- Any uses customarily incidental and accessory to the principal permitted uses(s) of the property.

**Parking**

Off-street parking shall be provided for all new development in the R-4 District.

- Off-street parking shall be accessed by a driveway not wider than twelve (12) feet.

- The following off-street parking shall be required.
  - One (1) space per unit for dwellings with one (1) bedroom.
  - One and one-half (1.5) spaces per unit for dwellings with two (2) bedrooms.
  - Two (2) spaces per unit for dwellings with three (3) or more bedrooms.

**Usable Open Space or Amenity Space**

All multiple family dwellings in the R-4 district shall provide a minimum of 150 square feet of exterior open space or interior amenity space per dwelling unit. These spaces may include, but are not limited to, landscaped outdoor areas with seating, spaces for gardening, rooftop terraces, balconies, fitness centers, and meeting rooms.

**Area, Yard, and Building Requirements**

See R-4 High Density Residential District's Bulk Standards Chart

**R-4 High Density Residential District Bulk Standards**

Minimum Lot Area	20,000 square feet
Minimum Lot Width	Two-hundred (200) feet.
Minimum Front Yard	Twenty-five (25) feet.
Minimum Side Yard (each)	Twenty (20) feet.
Minimum Rear Yard	Twenty (20) feet.
Maximum Building Height	Twenty-one (21) stories and two-hundred fifty (250) feet
Maximum Density	One-hundred (100) units/acre
Maximum Building Coverage	None
Maximum Impervious Surface Coverage	Twenty (20%) percent.

*R-4 Housing*



## SR SENIOR RESIDENTIAL DISTRICT

### Intent

The intent and purpose of this district is to provide for high-density mid-rise senior citizen apartment residential areas.

### Principal Permitted Uses

- Mid-rise senior citizen housing.

### Accessory Uses

- Private swimming pools and other similar recreational facilities.
- Fences and walls.
- Community Gardens.

### Conditional Uses

- Quasi-public non-profit clubs and organizations.

### Parking

Off-street parking shall be provided for all development in the SR District.

- Off-street parking shall be accessed by a driveway not wider than twelve (12) feet.
- The following off-street parking shall be required.
  - One (1) space per unit for dwellings with one (1) bedroom.
  - An additional half (0.5) space per unit

for each additional bedroom.

### Usable Open Space or Amenity Space

All multiple family dwellings in the SR district shall provide a minimum of 150 square feet of exterior open space or interior amenity space per dwelling unit. These spaces may include, but are not limited to, landscaped outdoor areas with seating, spaces for gardening, rooftop terraces, balconies, fitness centers, and meeting rooms.

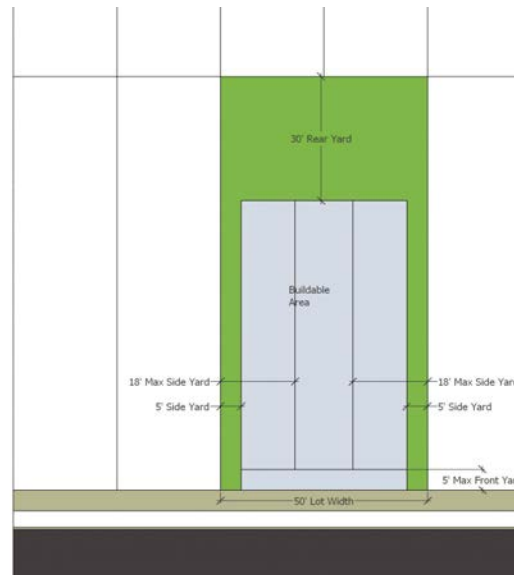
### Area, Yard and Building Requirements

See SR Senior Residential District's Bulk Standards Chart

### S-R Senior Residential District Bulk Standards

Minimum Lot Area	5,000 square feet
Minimum Lot Width	Fifty (50) feet
Minimum Front Yard	None
Maximum Front Yard	Five (5) feet
Minimum Side Yard (each)	Five (5) feet
Maximum Side Yard (either)	Eighteen (18) feet
Minimum Rear Yard	Thirty (30) feet
Maximum Building Height	Eighty-five (85) feet and seven (7) stories
Maximum Building Coverage	Eighty-five percent (85%)
Maximum Impervious Surface Coverage	None

SR Senior Residential Bulk Standards Diagram



Senior Residences

**AR ADAPTIVE REUSE DISTRICT****Intent**

The Adaptive Re-Use District is intended to provide for the re-use of existing historic buildings by adapting them to house residential units and compatible commercial uses. The intent of this district is to maintain the existing buildings to the greatest extent feasible, while allowing them to be utilized to meet community needs.

**Principal Permitted Uses**

- Multi-family residences.
- Retail stores and shops.
- Personal service businesses.
- Financial Institutions.
- Business or professional offices.
- Hotels.
- Restaurants (excluding drive-in or drive-thru restaurants).
- Public Facilities.
- Mixed Use Development of any combination of the principal permitted uses.

**Accessory Uses**

- Off-street parking
- Any uses customarily incidental and accessory to the principal permitted uses(s) of the property

**Parking**

- The following off-street parking shall be required.
  - Retail stores and personal service businesses: One (1) space per one-thousand (1,000) square feet.
  - Business and professional offices: One (1) space per one-thousand (1,000) square feet.
  - Financial institutions: One (1) space per one-thousand (1,000) square feet.
  - Restaurants: One (1) space per eight (8) seats.
  - Residences: One (1) space per residential unit.
  - Hotels: One (1) space per guest room.
- Off-street parking may be provided off-site on another parcel that is located no greater than three-hundred (300) feet from the property.

**Area, Yard, and Building Requirements**

See AR Adaptive Use District's Bulk Standards Chart

**AR Adaptive Reuse District Bulk Standards**

Minimum Lot Area	None
Minimum Lot Width	None
Minimum Front Yard <sup>1</sup>	Existing building footprint shall be maintained
Minimum Side Yard <sup>1</sup>	Existing building footprint shall be maintained
Minimum Rear Yard <sup>1</sup>	Existing building footprint shall be maintained
Maximum Building Height <sup>2</sup>	New construction may extend a maximum of two (2) stories or twenty-five (25) feet above the existing building roofline.
Maximum Building Coverage	Existing building footprint
Maximum Impervious Surface Coverage	None
1. New Construction or additions to the existing buildings may extend beyond the existing building footprint by no more than four (4) feet.  2. New construction above the existing roofline must be set back a minimum of ten (10) feet from the existing roofline on all sides.	

## C-1 NEIGHBORHOOD COMMERCIAL DISTRICT

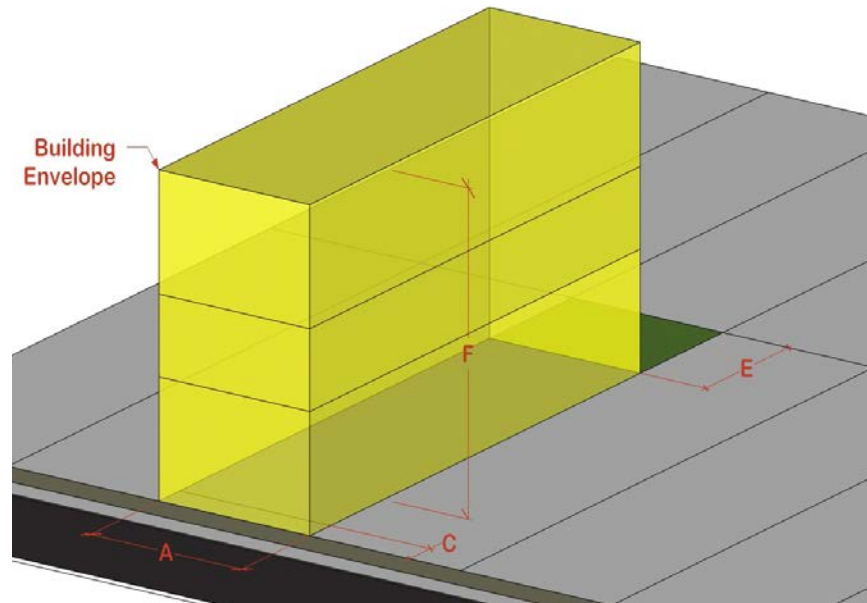
### Intent

The district is designed to provide retail and personal service businesses which supply the everyday needs of the neighborhood residents. The district permits retail and service operations which provide a convenience to area residents. The intent of the district standards are to allow for convenience retail and service businesses without disrupting the general small-scale residential character of the greater neighborhood area.

### Principal Permitted Uses

- Retail stores and shops (excluding liquor stores)
- Personal service businesses
- Restaurants (excluding drive-in or drive-thru restaurants)
- Banks and financial institutions (excluding drive thru service)
- Dry cleaning and clothing laundries occupying not more than 3,000 square feet of floor area; provided further that no central plant servicing more than one (1) retail outlet shall be provided. Any establishment of this nature which occupies more than 3,000 sq. ft. shall be considered an industrial establishment and shall be prohibited.
- Child care centers.
- Public facilities.
- Mixed-use development which includes residential uses and permitted neighborhood commercial uses:
  - Residential uses shall only be permitted above the ground floor of a building.

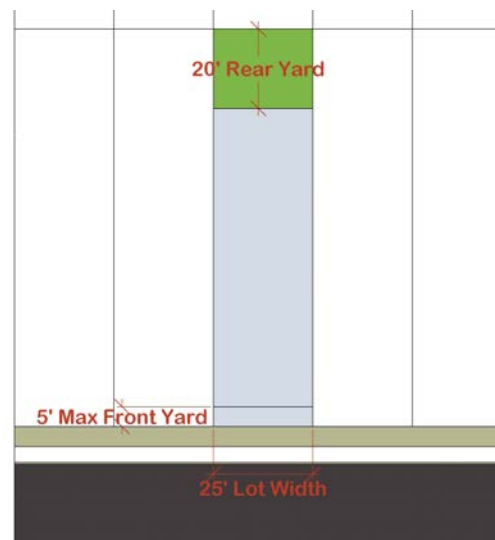
C-1 Neighborhood Commercial Bulk Standards Diagram



### Neighborhood Commercial

A	Minimum Lot Width
B	Minimum Front Yard Setback
C	Maximum Front Yard Setback
D	Minimum Side Yard Setback
E	Minimum Rear Yard Setback
F	Maximum Building Height

C-1 Neighborhood Commercial Bulk Standards Diagram





- Business and professional offices.
  - These uses shall only be permitted above the ground floor of a building.
- Travel Agencies and Real Estate offices.

buildings: One (1) space per dwelling unit.

### ***Area, Yard and Building Requirements***

See C-1 Neighborhood Commercial District's Bulk Standards Chart

### ***Accessory Uses***

- Any uses customarily incidental and accessory to the principal use of the property.

### ***Parking***

Lots with a width of fifty (50) feet or greater prior to subdivision and/or development shall provide off-street parking. No off-street parking is required on existing lots that are less than fifty (50) feet in width.

- The following off-street parking shall be required.
  - Retail stores, personal service businesses, and dry cleaning: One (1) space per six-hundred (600) square feet.
  - Financial institutions: One (1) space per six-hundred (600) square feet.
  - Restaurants: One (1) space per four (4) seats.
  - Business and professional offices: One (1) space per one-thousand (1,000) square feet.
  - Travel agencies and real estate offices: One (1) space per one-thousand (1,000) square feet.
  - Child care centers: One (1) space per employee on maximum shift.
  - Residential apartments in mixed use

### **C-1 Neighborhood Commercial District Bulk Standards**

Minimum Lot Area	2,000 square feet
Minimum Lot Width	Twenty-five (25) feet
Minimum Front Yard	None
Maximum Front Yard	Five (5) feet
Minimum Side Yard (each)	None
Minimum Rear Yard	Twenty (20) feet
Maximum Building Height	Forty (40) feet and three (3) stories
Maximum Building Coverage	Eighty percent (80%)
Maximum Impervious Surface Coverage	One-hundred percent (100%)



*Neighborhood Commercial Retail*

## C-2 GENERAL COMMERCIAL DISTRICT

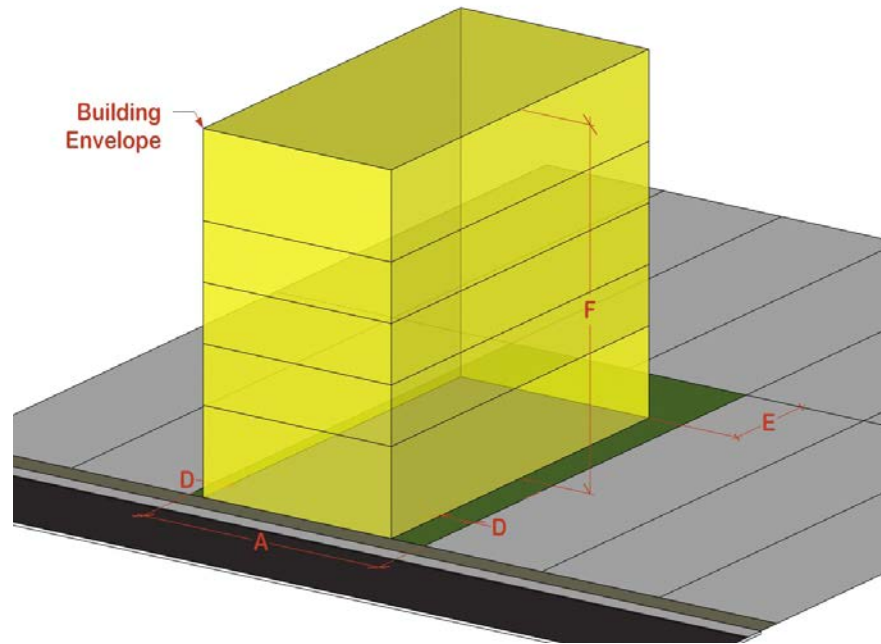
### Intent

The intent of the C-2 general commercial district is to provide sufficient space in appropriate locations for a wide variety of community commercial and service activities, serving a wider area than the C-1 district. This district is located normally at the conveyance of or along major thoroughfares where a general mixture of commercial and service activity now exists, but which uses are not characterized by extensive warehousing or frequent heavy truck activity.

### Principal Permitted Uses

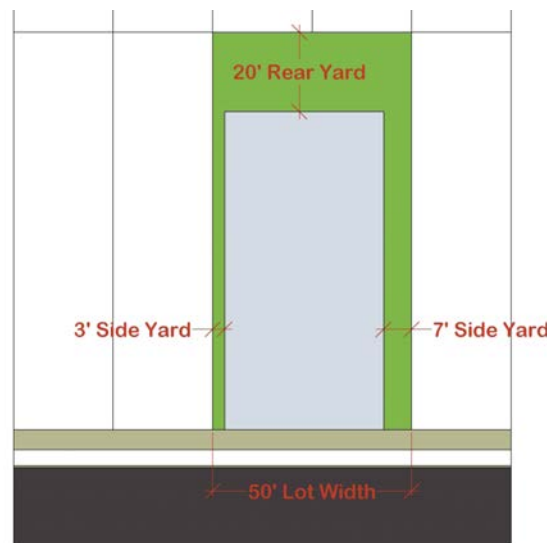
- Retail stores and shops.
- Personal service businesses.
- Business and professional offices.
- Art galleries.
- Restaurants (including drive-in or drive-thru restaurants)
  - Drive-thru restaurants must provide a designated drive-thru lane that does not obstruct pedestrian or vehicular traffic, with sufficient capacity for stacking of a minimum of five (5) vehicles.
- Banks and financial institutions
  - Drive-thru facilities are permitted provided that driveway space is provided off the street, which does not obstruct pedestrian or vehicular traffic, for five (5) or more vehicles waiting for drive-thru service.
- Dry cleaning and clothing laundries occupying not more than 3,000 square feet of floor

C-2 General Commercial Bulk Standards Diagram



General Commercial	
A	Minimum Lot Width
B	Minimum Front Yard Setback
C	Maximum Front Yard Setback
D	Minimum Side Yard Setback
E	Minimum Rear Yard Setback
F	Maximum Building Height

C-2 General Commercial Bulk Standards Diagram



General Commercial Business

area; provided further that no central plant servicing more than one (1) retail outlet shall be provided. Any establishment of this nature which occupies more than 3,000 sq. ft. shall be considered an industrial establishment and shall be prohibited.

- Funeral homes.
- Child care centers.
- Public facilities.
- Mixed-use development which includes residential uses and permitted commercial uses:
  - Residential uses shall only be permitted above the ground floor of a building.

#### **Accessory Uses**

- Any uses customarily incidental and accessory to the principal use of the property.

#### **Parking**

Off-street parking is required on for all development in the C-2 District.

- The following off-street parking shall be required.
  - Retail stores, personal service businesses, and dry cleaning: One (1) space per six-hundred (600) square feet.
  - Financial institutions: One (1) space per six-hundred (600) square feet.
  - Restaurants: One (1) space per four (4) seats.
  - Galleries: One (1) space per five-hundred (500) square feet of floor

area.

- Business and professional offices: One (1) space per one-thousand (1,000) square feet.
- Travel agencies and real estate offices: One (1) space per one-thousand (1,000) square feet.
- Funeral Homes: One (1) space per one-hundred (100) square feet of floor area in viewing rooms.
- Child care centers: One (1) space per employee on maximum shift.
- Residential apartments in mixed use buildings: One (1) space per dwelling unit.

#### **Area, Yard and Building Requirements**

See C-2 General Commercial District's Bulk Standards Chart

#### **C-2 General Commercial District Bulk Standards**

Minimum Lot Area	5,000 square feet
Minimum Lot Width	Fifty (50) feet
Minimum Front Yard	None
Minimum Side Yard (each)	Three (3) feet
Minimum Side Yard (both)	Ten (10) feet
Minimum Rear Yard	Twenty (20) feet
Maximum Building Height	Sixty (60) feet and five (5) stories
Maximum Building Coverage	Eighty percent (80%)
Maximum Impervious Surface Coverage	One-hundred percent (100%)





## ***PU PUBLIC USE DISTRICT***

### ***Intent***

The intent of this district is to create areas for the public and quasi-public uses located close to commercial centers, but still accessible to much of the neighborhood. The restoration of Hinchliffe Stadium and establishment of connections to the National Historical Park is a key goal.

### ***Principal Permitted Uses***

- Governmental facilities.
- Schools.
- Community centers.
- Museums.
- Stadiums and/or sports complexes
  - Baseball, football and soccer fields
  - Ice rinks, gymnasiums and natatoriums
  - Academic building – classrooms
- Recreation and Open Space

### ***Accessory Uses***

- Restrooms and concessions
- Restaurants and bars
- Administration and cafeteria
- Maintenance and vertical circulation building
- Off-street parking
- Any uses customarily incidental and accessory to the principal permitted use of the property



*Christopher Hope Center*



*Public School Number 4*



*Hinchliffe Stadium*



Waterfront Park, Hoboken NJ



Riverwalk Park, Wissahickon Creek PA



Delaware &amp; Raritan Canal, Lambertville NJ

## OS OPEN SPACE DISTRICT

### Intent

The intent of this district is to create a linear park and open space area adjacent to the Passaic River to serve residents of the entire City. This district is also intended to help provide a buffer between the residential neighborhoods in the Area, and the flood-prone riverfront properties.

### Principal Permitted Uses

- Open space
- Community recreation facilities
  - Athletic fields/courts
  - Community gardens
  - Community swimming pools
  - Walking/Bicycle trails
- Urban Farms
- Single-family Residences in accordance with the following bulk standards:
  - Minimum Lot Area: five-thousand (5,000) square feet.
  - Minimum Lot Width: fifty (50) feet.
  - Minimum Front Yard: twenty-five (25) feet.
  - Minimum Side Yard: four (4) feet.
  - Minimum Aggregate Side Yard: fourteen (14) feet.
  - Minimum Rear Yard: twenty (20) feet.
  - Maximum Building Height: two and one-half (2.5) stories and thirty-five (35) feet.
  - Maximum Lot Coverage: thirty (30%) percent.

### Accessory Uses

- Off-Street Parking

NPG NATIONAL PARK GATEWAY DISTRICT

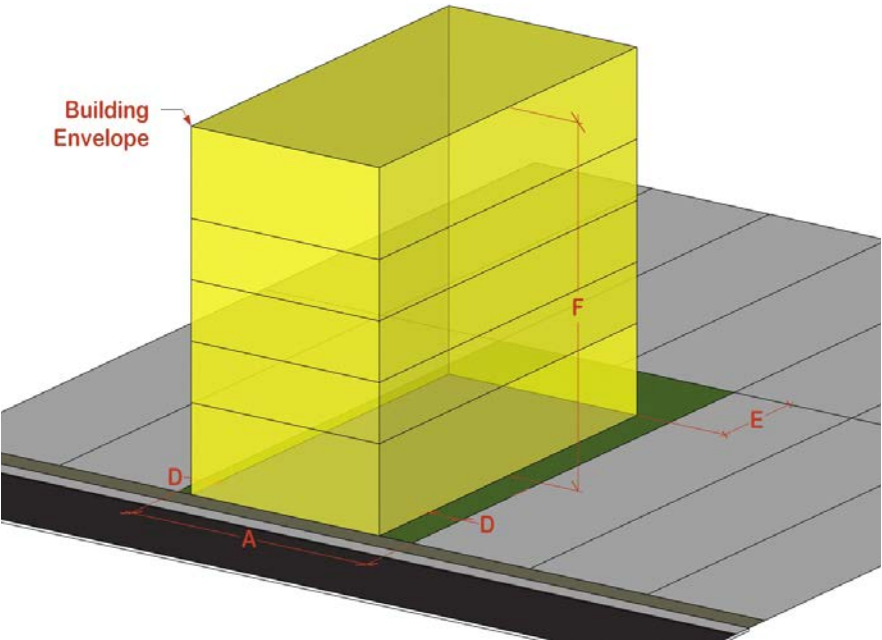
Intent

The intent of the National Park Gateway District is to create an attractive gateway into the Great Falls National Historical Park. The district permits a range of uses as the area provides a transition between the National Park and the commercial areas on West Broadway. The area is intended to feed into the National Park, and provide uses complementary to the Park. The Passaic River greenbelt should also be extended along the banks of the river to the Valley of the Rocks in the National Park, creating greater recreational access to the Park.

Principal Permitted Uses

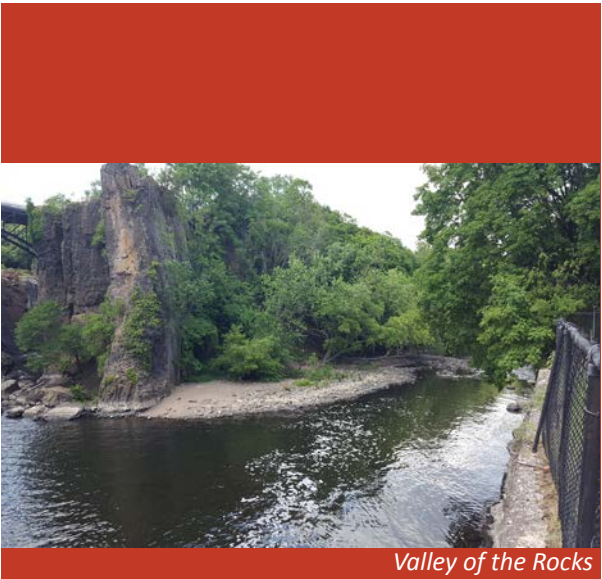
- Attached Single-family dwellings, townhouses
  - Maximum of four (4) units per building.
- Two-family dwellings.
- Three- to four-family dwellings.
- Multi-family residences.
- Community gardens.
- Retail stores and shops.
- Personal service businesses.
- Business and Professional offices.
- Art galleries.
- Restaurants (excluding drive-in or drive-thru restaurants).
- Bars.
- Hotels.
- Bed & breakfasts.
- Banks and financial institutions (excluding drive-thru facilities).
- Museums.
- Governmental facilities (including facilities

NPG National Park Gateway Bulk Standards Diagram



National Park Gateway	
A	Minimum Lot Width
B	Minimum Front Yard Setback
C	Maximum Front Yard Setback
D	Minimum Side Yard Setback
E	Minimum Rear Yard Setback
F	Maximum Building Height

NPG National Park Gateway Bulk Standards Diagram





operated by NPS)

- Public facilities.
- Mixed-use buildings containing any combination of permitted uses.
- Open space, recreational facilities, scenic overlooks, and walkways

### Accessory Uses

- Structured parking
- Surface parking lots.
- Any uses customarily incidental and accessory to the principal use of the property.

### Parking

Off-street parking is required for all development in the NPG District.

All parking in this district shall be located to the side or rear of any principal structure. No parking in the front yard shall be permitted.

- The following off-street parking shall be required.
  - Retail stores, personal service businesses: One (1) space per six-hundred (600) square feet.
  - Financial institutions: One (1) space per six-hundred (600) square feet.
  - Restaurants and bars: One (1) space per four (4) seats.
  - Galleries: One (1) space per five-hundred (500) square feet of floor area.
  - Business and professional offices: One (1) space per one-thousand (1,000)

square feet.

- Hotels and Bed & breakfasts: One (1) space per guest room, plus one (1) space per employee on maximum shift.
- All residences: One (1) space per dwelling unit.

### Area, Yard and Building Requirements

See NPG National Park Gateway District's Bulk Standards Chart

### Additional Requirements

Any property with frontage along the Passaic River shall be required to provide an extension of the Passaic River greenway, in the form of an easement of at least thirty (30) feet in width along the River.

Given the size of several of the properties within this district, development may occur with multiple principal structures on a single lot.

All development within the NPG District shall be reviewed with consideration of its impact on the Great Falls National Historical Park and its viewshed.

NPG National Park Gateway District Bulk Standards	
Minimum Lot Area	None
Minimum Lot Width	None
Minimum Front Yard	None
Maximum Front Yard	Ten (10) feet
Minimum Side Yard (each)	Three (3) feet
Minimum Side Yard (both)	Ten (10) feet
Minimum Rear Yard	Twenty (20) feet
Maximum Building Height	Sixty (60) feet and five (5) stories
Maximum Building Coverage	Sixty percent (60%)
Maximum Impervious Surface Coverage	Eighty percent (80%)



Mixed Use Commercial Streetscape, Mashpee MA

**LIVE/WORK COMMUNITY OVERLAY DISTRICT****Intent**

The intent of this district is to provide for the opportunity for development of a mixed-use, live/work community of creative entrepreneurs. This community would be constructed of small residential apartment units that are connected to small retail shops or production spaces in a live/work accommodation where the residents of the apartments work in the shops below their apartment.

**Principal Permitted Uses**

- Multi-family residences.
- Retail stores and shops.
- Personal service businesses.
- Professional offices.
- Medical and dental offices.
- Grocery stores.
- Restaurants.
- Bakeries and cafes.
- Micro-manufacturing and production spaces for goods to be sold or distributed on the premises.
- Artist's studios.
- Any other use similar in character and intensity to these principal permitted uses.
- Mixed Use Development of any combination of the principal permitted uses.

**Accessory Uses**

- Any uses customarily incidental or accessory to the principal uses.

**Parking**

- Off-street parking requirement:
  - Residences: One (1) space per four (4) residential units.
  - No off-street parking is required for any non-residential use of the property.
- Off-street parking may be provided off-site on another parcel that is located within three-hundred (300) feet of the property.
- No parking may be permitted in the front yard area.
- A minimum of one (1) bicycle parking space shall be provide per unit.

**Area, Yard and Building Requirements**

See Live/Work Community Overlay District's Bulk Standards Chart

**Other Requirements**

- The provisions of this overlay district shall only be applicable under the condition that live/work units are constructed, with a deed restriction that stipulates that the owner or operator of any non-residential use must reside in an apartment unit on the premises.
- A minimum of four (4) work/shop units must constructed on the site.
- Retail shops, restaurants, bakeries, and cafes must be located on the ground level.
- The ratio of apartments to work/shop spaces shall not exceed 4:1.
- All development within the underlying NPG District shall be reviewed with consideration of its impact on the Great Falls National Historical Park and its viewshed.

**Live/Work Community Overlay District Bulk Standards**

Minimum Lot Area	5,000 square feet
Minimum Lot Width	None
Minimum Front Yard	None
Maximum Front Yard	None
Minimum Side Yard (each)	None
Minimum Rear Yard	None
Maximum Building Height	None
Maximum Floor Area Ratio (FAR)	6.0
Maximum Building Coverage	Eighty percent (80%)
Maximum Impervious Surface Coverage	Eighty percent (80%)





# DESIGN STANDARDS

## PATERSON FIRST WARD





## GENERAL DESIGN STANDARDS

The purpose of the design standards for the First Ward Redevelopment Plan is to ensure that the quality of construction, and general design and aesthetic of all new construction meets certain minimum standards. These standards are meant to encourage design that is sensitive to the urban context of the First Ward of the City of Paterson, and does not in any way detract or denigrate the character of the existing community.

New construction or proposed renovations must maintain continuity in design with the larger physical context of the Area. Excessive contrast may overwhelm the surrounding existing structures. New construction or renovations should take cues from the surrounding buildings. Continuity in streetscape elements such as sidewalks, street trees, light fixtures, signs, and plantings is also desired.

### General Residential Design Standards

Paterson's housing stock is eclectic, featuring an array of housing types and architectural styles. While no one style predominates, the best housing in Paterson is of a traditional urban pattern characterized by front porches, small setbacks, vertical windows, sloping roofs, traditional materials, coordinated color schemes, and architectural features such as gables and dormers. These properties also contain amenities such as street trees and ornamental fencing. All new housing should emulate and conform to this traditional urban pattern.

While houses should be based on the traditional urban pattern, there should be some variety in the

forms that patterns take. As such, there should be a variety of roof and porch styles, window groupings, façade colors, and façade details such as shutters and sash trim. New construction or renovations should strive to strike a balance between consistency and variety. Housing should be oriented to the existing street network and should generally follow traditional urban design principles.

### Encouraged

- Hedges to help delineate public/private space
- High quality ornamental wooden/metal fencing
- Street trees
- Shutters
- Sitting walls/porches/stoops
- Attractive color schemes
- Landscaping
- Architectural details such as motifs, cornices, and window frame details are encouraged.
- Porches or stoops to create a physical and psychological separation between the private space of the building and the public space of the street.

### Discouraged

- Barred windows.
- Lack of porches/stoops.
- Lack of delineation between public/private space. (fencing, hedges, etc.)
- Chain link fences.
- Front yard parking.
- No street trees, minimal landscaping.



*Appropriate Residential Design*



*Poor Residential Streetscape Design*

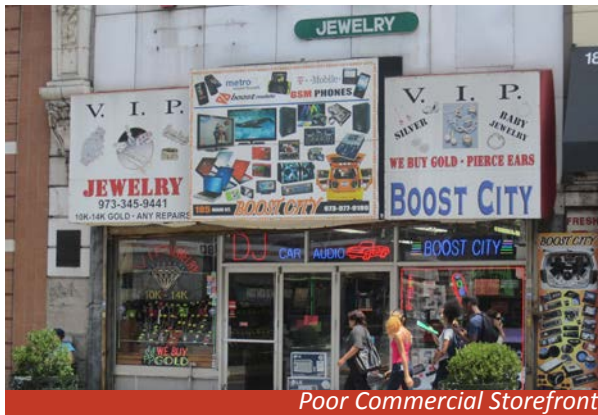


*Attractive Residential Streetscape*





*Appropriate Mixed Use/Commercial Building*



*Poor Commercial Storefront*



*Poor Commercial Storefront*

### ***General Commercial Design Standards***

These standards are intended to enhance and reinforce the physical and visual characteristics of the commercial districts in the First Ward Redevelopment Area. They are to be used in conjunction with the zoning and bulk standards that are established in this Redevelopment Plan. The general façade design standards are to be applied to renovations of existing structures, infill construction and new construction on vacant property.

The intent of these standards is to encourage a lively and attractive retail shopping atmosphere in front of commercial buildings.

### ***Encouraged***

- Visual coordination of building elements.
- Stores built at lot lines framing pedestrian activity on sidewalk.
- Open and transparent retail storefronts.
- Cornice and window lines coordinated with adjacent buildings.
- Awning lines matching in old and new construction.
- Security gates designed to blend into the streetscape.
- Security gates installed on the inside of entrance doors rather than the outside.

### ***Discouraged***

- Excessive signage.
- Windows and storefronts covered with signage or blocked by merchandise.
- Blank or featureless walls.

- Obtrusive security grates

## RESIDENTIAL ARCHITECTURAL DESIGN STANDARDS

This section governs the architectural design of all residential structures in the First Ward Redevelopment Area.

### Permitted Exterior Building Materials

- Brick
- Natural stone
- Wood (pressure treated or naturally decay resistant)
- Stucco
- EIFS (above the first floor only)

### Windows and Doors

- Openings for windows and window panes shall have a vertical dimension that is greater than the horizontal dimension.
- If exterior shutters are used, they shall be sized appropriately to match the window opening.
- Windows may be grouped to create a horizontal composition, with a maximum of three windows grouped together.
- Windowsills shall project a minimum of 2 inches from the building face.
- Windows shall not be placed flush with the exterior building wall, but shall be centered within the wall cavity.
- Where siding is used, all openings for windows or doors shall be cased. Trim boards must have a minimum nominal dimension of 1"x4" on sides and sill, and a nominal dimension of 1"x6" for the head.

- All header trim around openings shall be at least 2 inches taller than the width of the adjacent trim.
- All glazing shall be at least 70% transparent.

### Residential Roofs

- All Single and Two Family residential structures must have pitched or sloped roofs.
- All hipped and gabled roofs shall have eaves that overhang a minimum of 18 inches from the building face.
- A hipped or gabled roof shall have a minimum pitch of 8:12.
- Dormers shall light habitable spaces, and if gabled, shall have a pitch to match that of the principal roof structure.
- Roof and wall penetrations, other than chimneys, shall be concealed from public view to the maximum extent feasible.
- Where a building has a flat roof, a cornice shall be included at the top of the building that projects a minimum of 1 foot from the building face.

### Other Design Standards

- Exterior building walls shall be consistent in architectural quality and detail on all elevations visible from the public street.
- Foundation exposures shall not be permitted in the front of a building.
- Porches and stoops shall be a minimum of 2 feet above the adjacent sidewalk elevation.
- If covered, stoops and porches shall be covered by a high quality roofing material,

such as metal, slate, concrete, or clay tile.

- Porches and stoops shall not be screened or otherwise enclosed.
- The foundation walls of porches and stoops shall be finished to match the exterior of the building.
- Projecting elements on the building face such as bay windows or balconies may encroach up to 4 feet into the front yard setback beginning above the first floor.



*Sloped/Pitched Roofs*



*Front Porches on Residences*

## RESIDENTIAL ARCHITECTURAL DESIGN STANDARDS



### *ENCOURAGED*

- Consistent scale and massing.
- Front porches create transition space between public realm and house.
- Varied building materials and window configurations.
- Varied roof forms.
- Dormer windows provide light into upper half story.
- Traditional residential design.



### *DISCOURAGED*

- Parking in front of residences.
- Lack of porches/stoops to delineate public and private spaces, and create transition space.
- Homes set back from street.
- Chain link fencing in front yard.



## COMMERCIAL ARCHITECTURAL DESIGN STANDARDS

This section governs the architectural design of all commercial structures in the First Ward Redevelopment Area. Where a mixed use building is proposed, the commercial design standards shall apply for the commercial portions of a building, while the residential standards shall apply for the residential portions of a building.

### Permitted Exterior Building Materials

- Brick
- Natural stone
- Wood
- Stucco
- Concrete (smooth or stucco finish)
- Steel and glass
- EIFS (above the first floor only)

### Retail Fenestration

- A minimum of 50% of the ground floor wall surface facing the public street shall be glazed.
- Glazing shall be a minimum of 70% transparent.
- A maximum of 20% of the glazing on storefronts and/or doors may be blocked by window signs or advertisements.
- Collapsible grill doors are prohibited from being used on the outside of a storefront or entrance unless partially transparent or painted with a mural approved by the Board.

### Commercial Roofs

- Where a building has a flat roof, a cornice shall be included at the top of the building that projects a minimum of 1 foot from the building face.
- Buildings with flat roofs shall have a parapet along the entire front façade that extends a minimum of 2 feet above the roofline.
- Where a commercial building has a sloped roof, the residential design standards for sloped roofs shall apply.
- Roof and wall penetrations, other than chimneys, shall be concealed from public view to the maximum extent feasible.
- Mechanical equipment located on a roof shall be screened from public view.

### Other Design Standards

- Awnings, marquees, canopies, and similar permitted design elements may project up to 4 feet into the front yard setback at a height of at least 8 feet above sidewalk grade.
- Awnings shall have a metal frame, and be covered by canvas or a like material.
- Exterior building walls shall be consistent in architectural quality and detail on all elevations visible from the public street.
- Foundation exposures shall not be permitted in the front of a building.



*Building Cornice and Parapet*



*Commercial Awning*

## COMMERCIAL ARCHITECTURAL DESIGN STANDARDS



### ENCOURAGED

- Consistent building scale and massing.
- Cornice and window lines coordinated.
- Varied building materials and configuration.
- Large, open windows for commercial/retail storefront.
- Mixed use of buildings with residences or office space above commercial uses.
- Storefront designed with signage area.
- Buildings front on public Right-of-Way, creating a sense of enclosure on the street.



### DISCOURAGED

- Lack of open storefront.
- Excessive signage.
- Windows blocked by merchandise and signage.

## STREETSCAPE STANDARDS

### *Residential Streetscapes*

Residential streetscapes are defined as both the public and private spaces located between the edge of the curb to the front of a house or dwelling. The quality of streetscapes goes a long way towards defining the character of a neighborhood. Neighborhoods with large trees and leafy front gardens make residents feel better. Neighborhoods with quality streetscapes encourage more people on the street and are therefore inherently safer. Properties located in attractive, safe neighborhoods generally have higher value and are more desirable to live in.

Paterson has many fine residential neighborhoods and attractive streetscapes located throughout the city. These residential streetscape standards seek to emulate the basic design principles that make these neighborhoods so attractive and comfortable and apply them throughout this Redevelopment Area.

Streetscapes have both a public and private realm. The public realm extends from the outside edge of the curb line to the edge of the public right of way, which usually corresponds with the inside edge of sidewalk. This realm includes the curb, planting strip and sidewalk and usually contains public amenities such as street trees, light poles, telephone poles and mailboxes.

The private realm extends from the edge of the public right of way to the front of a house or dwelling. This realm is where the private house meets the public street. Some houses may be set

flush with the right of way and not have any private space, while others may be set back quite a bit and contain porches, driveways, fences, walls, and/or landscaping.

### *Residential Streetscape Standards*

- Sidewalks shall be constructed on both sides of all public streets.
- Sidewalks shall be a minimum of 5 feet in width.
- Sidewalks shall be constructed of a material that is different from that of the roadway.
- A planting strip for street trees of at least 3 feet in width shall be located adjacent to the sidewalk, between the sidewalk and the curb.
- The curb between the planting strip and the street shall be a minimum of 6 inches in width, and a maximum of 8 inches in width.
- Garden walls in the front yard are permitted to be no taller than 3 feet in height.
- Front yard setback areas shall be landscaped with a combination of appropriate flowering trees, shrubs, perennials, and/or low-maintenance groundcovers.
- Front lawns and turf are discouraged where the front yard setback is less than 5 feet.
- Efforts shall be made to make utilities as unobtrusive as possible.
- Transformers, generators, and other mechanical equipment shall not be located on the sidewalk, or within the required front yard area.
- Utilities shall be located underground, unless site specific constraints prohibit this.

### *Commercial Streetscapes*

Commercial streetscapes are defined as the area between the outside edge of the curb to the front of a building. Unlike buildings in residential areas, commercial buildings are usually set flush with the edge of the right of way, with little or no setback. Even with minimal areas for landscaping or planting, commercial streetscapes can be enhanced to create attractive, safe pedestrian environments. The parts of businesses visible from the street for commercial uses should be designed to be as attractive and inviting as possible.

Retail businesses contribute to the streetscape by how they are experienced at the pedestrian level. Businesses that are clean, open, and visually interesting positively reinforce the streetscape. Businesses that are cluttered, shuttered, and lack amenities discourage pedestrian traffic. The major features that define the streetscape in a retail environment are windows, signage, awnings, and lighting. It should be noted that retail businesses with large, open windows, decorative lighting, and simple awnings are more desirable and attract more business than retail buildings without.

### *Commercial Streetscape Standards*

- Crosswalks shall be a minimum of 5 feet in width, and located on all sides of all intersections of roads where commercial uses are present, or permitted.
- Sidewalks shall be provided to all entrances of all buildings.
- Sidewalks shall be constructed of a material that is different from that of the roadway.
- Sidewalks shall be a minimum of 8 feet in



# STREETSCAPE DESIGN STANDARDS



## RESIDENTIAL STREETSCAPE

- Planted landscape buffer strip between street and sidewalk.
- Front porches open to street.
- Small, landscaped front yards rather than expansive lawns.
- Street trees along both sides of street, providing shade.



## COMMERCIAL STREETSCAPE

- Paver strip delineates sidewalk area from street.
- Street trees planted within paver strip, covered with tree grates.
- Bicycle parking provided.
- Buildings and storefronts line the street.
- Large open storefront windows.
- Outdoor seating and pedestrian amenities provided.
- Street lights spaced between street trees to avoid blockage of light.

width.

- A planting/paver strip for street trees and alternate paving materials of at least 3 feet in width shall be located adjacent to the sidewalk, between the sidewalk and the street.
- The curb between the planting strip and the street shall be a minimum of 6 inches in width, and a maximum of 8 inches in width.
- Front yard setback areas shall contain a combination of hardscape (sidewalk) and landscaped areas.
- Street furniture such as benches, planters, or newspaper boxes shall be located within either the front yard area, or the required planting/paver strip. Sidewalks shall not be impeded by street furniture.
- Trash and recycling receptacles for public use shall be located near all intersections within commercial use areas.
- Outdoor storage is not permitted in the front yard area.
- All outdoor storage in side or rear yard areas must be screened from public view with a solid fence.
- Efforts shall be made to make utilities as unobtrusive as possible.
- Transformers, generators, and other mechanical equipment shall not be located on the sidewalk, or within the required front yard area.
- Utilities shall be located underground, unless site specific constraints prohibit this.
- Outdoor dining shall be permitted on the sidewalk in front of restaurants, cafes, and other eating and drinking establishments provided that a minimum effective sidewalk width of 5 feet is maintained.

- Outdoor dining shall not block or impede any vehicular or pedestrian entrances.
- Restaurants must provide for the disposal of waste and recyclable materials on their own. Public garbage and recycling receptacles on the street shall not be used by restaurants or their patrons.
- All food preparation for outdoor dining shall be conducted within the regular kitchen area of the restaurant.

### ***Complete Streets & Green Streets Standards***

In addition to the traditional streetscape standards listed above, developers should be encouraged to pursue environmentally friendly and sustainable “green” streetscape designs that encourage pedestrian activity, help to mitigate stormwater run-off, and provide a more attractive environment. “Complete Streets”, or streets that take into account the needs of multiple users using multiple modes of transportation, should also be strongly encouraged.

An NJDEP 319h grant was awarded to the City to implement a Green Street design on Haledon Avenue between the Passaic River and N. 2nd Street. This project, and the Passaic County Complete Streets design standards are consistent with the goals and standards contained in this plan. Broader implementation of both concepts is encouraged.

The following complete street and green street improvements should be implemented to the



*Outdoor Dining, Hoboken NJ*



*Street Tree Planting Strip, Washington DC*



*Curb Extension, Mashpee Commons MA*





*Designated Bicycle Lane, Pottstown PA*



*Vegetated Stormwater Inlet, Philadelphia PA*



*Curb Inlet and Vegetation, Philadelphia PA*

greatest extent feasible.

- In commercial areas, provide curb extensions or bump-outs at all crosswalks. Curb extensions reduce the overall length of a crosswalk, and extend the sidewalk into the street area, which makes crossing the street safer for pedestrians.
  - Landscaping of curb extensions should be encouraged.
- Travel lane widths for vehicles should be minimized to reduce the speed at which vehicles travel, and provide space for other amenities. This increases the safety of the streets for pedestrians, bicyclists, and other users.
- Landscaped medians should be included where space is available on major corridors such as Haledon Avenue, and West Broadway.
- Where street width is available, designated bicycle lanes should be striped into the street. This will help to encourage bicycling as a viable form of transportation, and reduce dependence on cars.
- Where adequate street width is not available for designated bicycle lanes, signs or markings indicating that travel lanes shall be shared by both automobiles and bicycles should be posted.
- Where adequate space is available, bus loading zones should be separated from travel lanes and parallel parking areas.
- Shelters for passengers should be located at all bus stops along bus routes.
- ADA accessible ramps should be provided at all intersections.
- Green Stormwater infrastructure should be utilized to reduce the impacts of storms

on the area storm-sewer system. Green stormwater infrastructure utilizes natural bio-filtration processes to collect water rather than channeling water directly to the storm sewer.

- Curb inlets, or breaks in the curb line, should be provided to allow for some stormwater to infiltrate in areas where vegetation or a street tree is beside the curb. This can reduce the amount of water that flows to the sewer system.
- Vegetated bio-swales along the sidewalk area should be provided where space is available. In lieu of simply a planter strip between the curb and the sidewalk, a vegetated bio-swale planted with vegetation that directly receives stormwater from the street should be encouraged.
- Rain gardens should be encouraged in front yard areas or side yard areas of residences.
- The use of pervious paving materials for sidewalks and driveways shall be encouraged.
  - Pervious concrete
  - Grass-Crete
  - Interlocking pavers



## LANDSCAPING STANDARDS

Landscaping is an important component of the overall streetscape environment, both in residential and commercial settings. Street trees, plantings, fences, and other landscaping can make the difference between an attractive streetscape, and a harsh one. The intent of the landscaping standards is to encourage appropriate landscaping in the form of street trees, shrubs, flowering plants, fences, and other features within the public and private realms of the streetscape.

### Street Trees

- Street trees shall be placed within the required planting strip between the sidewalk and street, and spaced at a maximum of 50 feet on/center.
- No more than 50% of the trees on any single block may be the same species of tree.
- All street trees shall be planted within metal tree grates, or a system designed to allow for pedestrians to pass over the tree.
- Street trees shall be a minimum of 2.5 inches in caliper.
- Recommended street trees are:
  - Red Maple
  - Autumn Cherry
  - Okame Cherry
  - Red Oak
  - Little leaf Linden
  - Chinese Elm
  - Japanese Zelkova
- Trees may be located in the front yard area of a residential property if there is a setback of at least 6 feet.
- Front yard trees shall be a minimum of 2.5

inches in caliper.

- The following trees are recommended for front yard areas:
  - Shadblow Serviceberry
  - Eastern Redbud
  - Cherry Dogwood
  - Kousa Dogwood
  - American Dogwood
  - Common Witchazel

### Shrubs and Plantings

- Placement of planters is encouraged in window sills of residences or stores, and near entrances to buildings.
- Wall mounted or floor mounted planters shall be located so that they do not hinder pedestrian movement along the sidewalk.
- Residential shrubbery shall be a minimum size of 3 gallon container, 18 – 24 inch B&B (balled & burlapped)
- Recommended residential shrubs:
  - Evergreen:
    - Dwarf barberry
    - Japanese holly
    - Blue holly
    - Juniper “Old Gold”
    - Japanese Pieris
    - Cherry Laurel
    - Nigra Arborvitae
  - Deciduous
    - Feather Reed Grass
    - Red Twig Dogwood
    - Burning Bush
    - Japanese Fothergill
    - Common Hydrangea
    - Oak Leaf Hydrangea



Street Trees, Alexandria VA



Metal Tree Grate



Street Plantings and Furniture

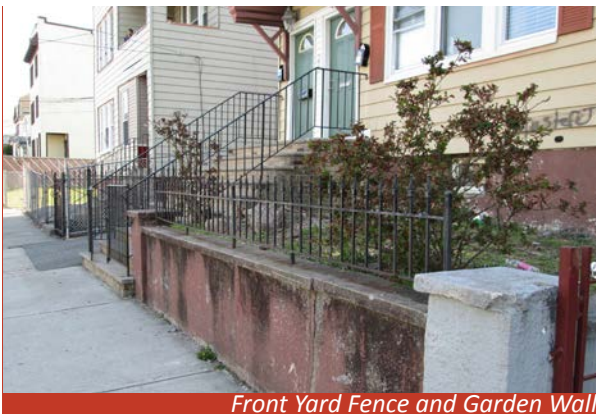


- Winterberry
- Maiden Grass
- Switch Grass
- Fountain Grass
- Koreanspice Viburnum
- Groundcovers shall be a minimum size of 1 gallon container
- Recommended groundcovers:
  - Lillyturf
  - Japanese Garden Juniper
  - Blue Star Juniper
  - Pachysandra
  - Daylily
  - Hosta



### ***Fencing***

- Fences in a front yard area must be visually open, and no greater than 4 feet in height.
- Chain link fences are prohibited in the front yard area.
- Opaque Board on board fences are prohibited in the front yard area.
- Fences must be of wood or high quality material.
- Fences shall be no higher than 6 feet.





## SIGNAGE STANDARDS

This section governs the number, type, and size of signs permitted on commercial buildings within the First Ward Redevelopment Area. Signage is a vital piece of commercial business operations, as it signals to potential customers the type of products or services being offered. Attractive signage not only encourages business for that particular location, but contributes to the overall visual environment of the entire streetscape and neighborhood. Signage that is too large or too cluttered with information can overwhelm the visual realm of the streetscape. Signage should be coordinated with the architecture of the building, and with the context of the other surrounding buildings.

### Signage Regulations

- No signs are permitted for residential properties other than signs indicating the street address.
- Commercial buildings shall be designed so as to include a “signage zone” integrated into the commercial storefront design. The signage zone shall be located above the ground level storefront.
- Commercial signage is permitted to have a maximum area of 10% of the ground floor street facing façade area, or 50 square feet, whichever is less.
- Each commercial use is permitted to have a maximum of two signs, the total of which shall not exceed the maximum permitted sign area.
- Sign area shall be calculated as the smallest rectangle enclosing all design elements of the

sign.

- No sign shall project above the roofline of a building.
- Wall signs or building mounted signs shall not project further than 6 inches from the building façade to which they are attached.
- Blade signs shall be a minimum of 8 feet above the ground, and shall have a maximum area of 6 square feet.
- Blade signs may project no further than 3 feet from the face of the building to which they are attached.
- Window signs shall not be permitted to occupy an area greater than 20% of the surface of the window to which they are attached.
- Directional signage indicating the location of entrances, parking, or other vital information shall be permitted as deemed necessary to provide directions. Directional signage shall not exceed 4 square feet per sign.
- Signage on awnings is permitted to include text only, and no graphics.
- Awning signs are permitted to occupy a maximum of 20% of the surface area of the awning.
- Signs may be internally illuminated, or externally illuminated by gooseneck light fixtures.

### Permitted Sign Types

- Wall Signs or Building Signs
- Blade Signs
- Window Signs
- Wall Murals (on existing blank walls only)

### Prohibited Signs

- Freestanding Signs
- Ground mounted signs
- Flashing signs
- Off-premises signs



Blade Sign



Retail Wall Sign and Awning





*Bicycle Parking, New Brunswick NJ*



*Parking Lot Buffer Plantings, Plainsboro NJ*



*Front Yard Parking, Not Permitted*

## **PARKING AND CIRCULATION STANDARDS**

The location and availability of parking within established commercial areas and residential neighborhoods is often very challenging. Parking must be provided for, but it must not overwhelm and degrade the pedestrian orientation of these areas. There will always be a limited amount of on-street parking and provisions must be required to provide off-street parking opportunities. Developers are required to provide on-site parking whenever possible, such as in new construction. However in situations, such as renovations of existing buildings, that do not contain parking spaces, developers are encouraged to develop off street parking arrangements in shared lots.

On-street parallel parking is provided on most of the streets within the Redevelopment Area, and may be taken into consideration in any development application.

The guidelines contained in this section of the Redevelopment Plan pertain to circulation and parking. The circulation and parking concerns of this Redevelopment Area were analyzed and compared with similar concerns in other Redevelopment Areas and the City as a whole.

### **Residential Off-Street Parking**

- Residential driveways shall have a minimum width of 9 feet.
- Residential parking garages shall be located to the rear of the principal structure.
- Parking shall not be permitted in the front yard area of a residence.

### **Commercial Off-Street Parking & Circulation**

- All 90 degree parking spaces shall have a minimum width of 9 feet, and a minimum depth of 18 feet where drive aisles are less than 24 feet in width.
- Where a drive aisle of at least 24 feet in width is provided, 90 degree parking stalls may have a width of 8.5 feet.
- All two-way drive aisles within parking areas shall have a minimum width of 22 feet.
- One-way drive aisles in parking areas shall have a minimum width of 12 feet.
- Parking lots shall be located no closer to a building than 3 feet.
- All surface parking lots that abut the public street shall be separated from the street by a buffer with a minimum width of 3 feet. Buffers shall include fencing, shrubberies, plantings, or a row of trees.
- Where multiple businesses are located in close proximity, shared parking arrangements should be encouraged.
- Driveways and curb cuts for surface parking lots should be minimized to reduce impacts to pedestrian circulation.
- No access driveway shall be located closer than 30 feet to another commercial parking lot access driveway.
- Bicycle storage racks should be located near the principal entrance to all commercial buildings.
- Bicycle storage should be provided at a minimum rate of 1 storage space for every 500 square feet of building area for commercial uses.

### ***Landscaping, Lighting, and Pedestrian Access of Parking Areas***

- Where a parking lot has 10 or more parking spaces, a minimum of 10% of the surface area shall be landscaped.
- Landscaping may be in the form of a hedge or shrubberies 3 feet to 4 feet in height, or a row of trees.
- Every parking lot shall provide a minimum of 1 tree for every 10 parking spaces. Trees may surround the parking lot or be located interior to the parking lot in tree islands.
- Parking lots that are visible from the public street shall not be enclosed by a chain link fence.
- Parking lots that have 30 or more parking spaces shall provide a pedestrian pathway or sidewalk of at least 5 feet in width through the parking lot area to public sidewalk, or to the primary building entrance.
- Pedestrian pathways/sidewalks within a parking lot shall be made of a different surface material than the parking areas of the lot, and are encouraged to include landscaping around the edges of the walkway.
- All parking lots shall be adequately illuminated to provide for the safety and security of users.



*Parking Lot Landscaping, Asbury Park NJ*



*Landscaped Parking Lot, Pottstown PA*



*Landscaped Parking Lot, South Orange NJ*



*Decorative Streetlight*

### **LIGHTING STANDARDS**

This section governs street lights, buildings lights, and other fixtures that will provide lighting of the public realm and streetscape environment within the First Ward Redevelopment Area. Public spaces must be adequately illuminated for the safety, security, and comfort of all people who may pass through them.

- Street light fixtures shall be mounted at a height of no greater than 18 feet. The supporting pole of the fixture shall have an overall height no greater than 20 feet.
- Street light illumination shall be consistent throughout a street area.
- The location of street lights shall be coordinated with the locations of street trees to avoid having trees block the lights.
- Building mounted lights shall be integrated into the overall architectural design of the building.
- All building entrances shall be illuminated by exterior lighting.
- Neon lighting is prohibited on commercial storefronts.
- Flashing lights are prohibited.

*Building Mounted Lighting*

### **CRIME PREVENTION THROUGH ENVIRONMENTAL DESIGN (CPTED) PRINCIPLES**

Crime Prevention Through Environmental Design (CPTED) is a set of design principles that seeks to make safe places that inhibit the likelihood of criminal activity. These goals are achieved through the combination of design principles, resident and business owner education and buy-in, and code enforcement.

An audit of the East Main Street and North Main Street corridors was conducted through a Together North Jersey initiative in 2015. This audit looked at the conditions in the neighborhood, identified hot spots for criminal activity, and made a series of recommendations, which are incorporated in this plan to the extent feasible.

The main findings of the audit were that pedestrian facilities tend to be inadequate and dangerous, vacant and abandoned buildings, late-night bodegas, take-out restaurants, and liquor stores tend to correlate with hot spots for criminal activity, lighting, landscaping, and fencing need to be improved, and there is a lack of high quality public space.

### **Natural Surveillance**

Natural surveillance design of public spaces makes it feel as if there are “eyes on the street”. Providing natural surveillance serves as a form of self-policing that functions as a crime deterrent and provides people a sense of security.

- Buildings should be oriented towards the public street, with windows and doors facing the street, to create a sense of “eyes on the



street”

- There should be a good visual connection between the public spaces of the street, and ground level spaces of buildings on those streets. Blank walls should be avoided.
- The public realm should be visually open and clear of barriers. Any place where there is a potential hiding spot or an area that is out of view from the street or public areas creates a potentially hazardous and unsafe space.
- Landscaping such as shrubs or bushes should not create a visual barrier.
- Fences in front yards should not exceed four feet and should be open. Chain link fencing is not desirable.

### Lighting

Sidewalks, building entrances, and public spaces should be well illuminated at night. People feel safer if they can see what is ahead of them, and around them.

- Ensure all building entrances are well lit.
- The exterior of buildings along public streets and spaces should also be lit.
- Public street lighting at a pedestrian scale should be provided, particularly at intersections, driveways, and crosswalks.

### Maintenance and Code Enforcement

Conducting regular maintenance and repairs on a building or space, and keeping it clean and litter free, gives the impression that someone cares about that space, and has the means to control it. Areas that are dirty and show serious signs of

dilapidation or disrepair indicate that there is little concern for the area, and that no one is asserting any ownership or control over the space. This leads to the impression that deviant or criminal behavior may be permissible in that space.

- Spaces should be designed to be low-maintenance, and easy to repair or clean.
- City code enforcement officials should offer a presence and enforce standards within this plan and the City General Code.

### Creating Temporary and Permanent Public Space

Public and semi-public spaces such as parking lots should be designed in such a way to make residents comfortable and make the use of these spaces attractive. Public art such as murals or street tattoos can create community pride and culture and instill ownership of a space. Programmable space that can host cultural events, farmers markets and other temporary or seasonal activities should be permitted and encouraged.

### Eliminating Crime Hot-spots

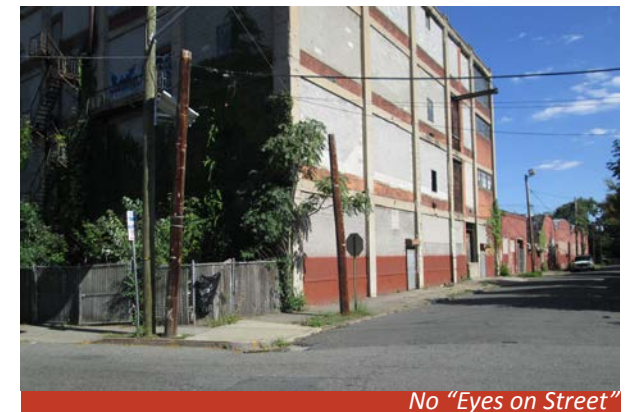
Neighborhood commercial zones should provide uses that support the community and are desirable. The CPTED Audit conducted in 2015 showed that criminal activity tends to be concentrated around vacant and abandoned properties and uses that keep late hours of operation such as bodegas, liquor stores, and take-out restaurants. This plan does not permit these uses in its residential districts and neighborhood commercial corridors.



*Large opaque fence obstructs vision of pedestrians*



*Blank Wall facing Street*

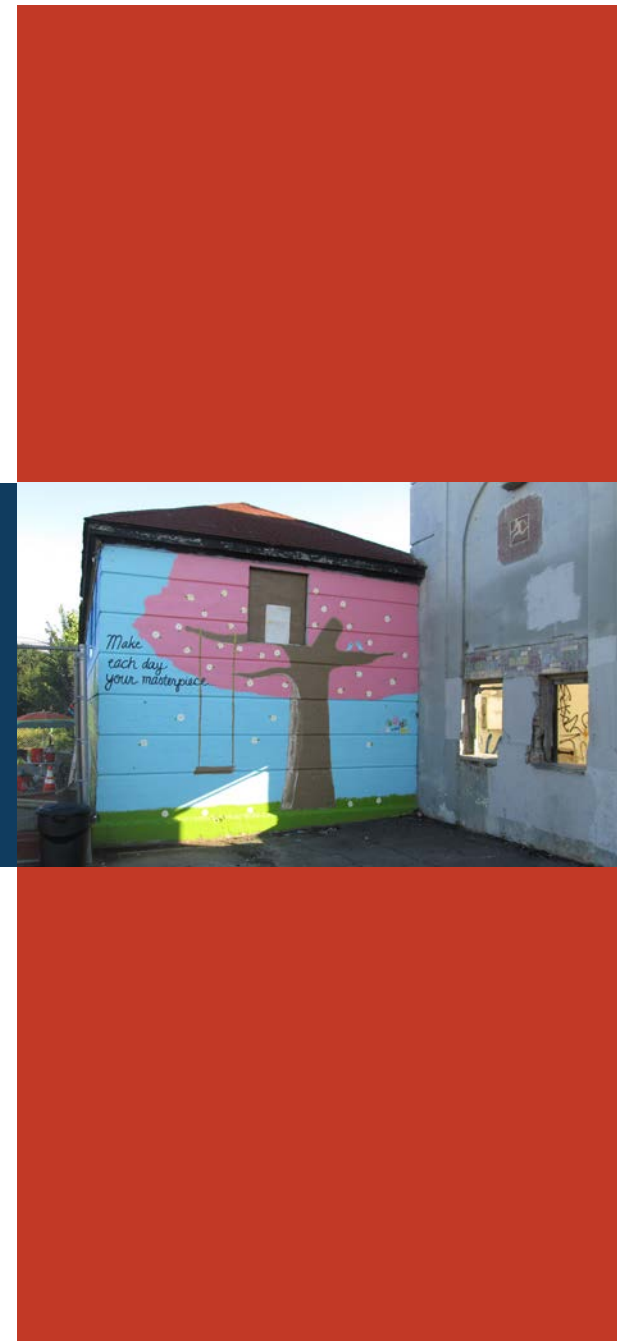


*No “Eyes on Street”*



# ACTION PLAN

## PATERSON FIRST WARD

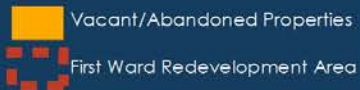




City of Paterson, NJ

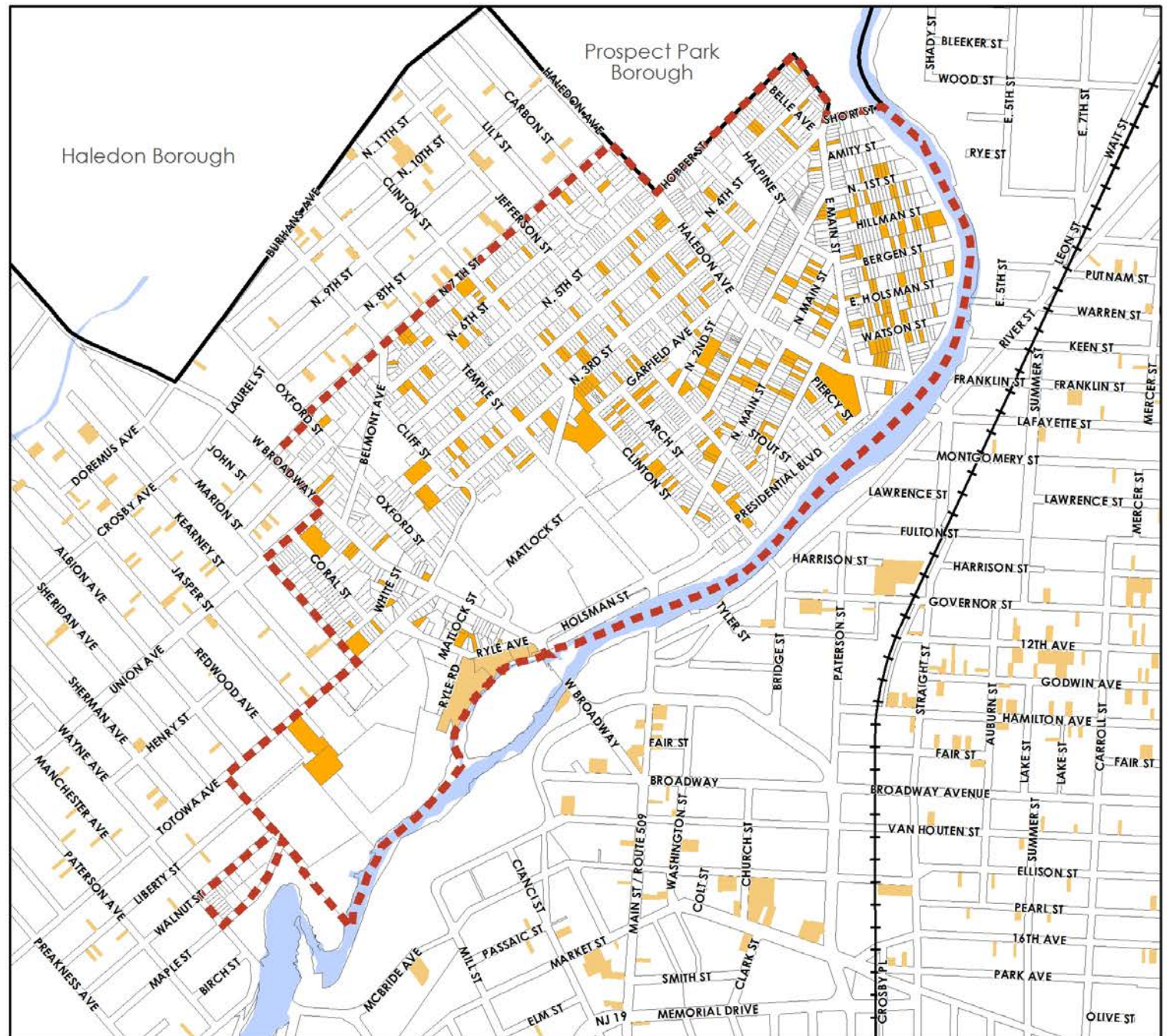
## First Ward Redevelopment Plan

## Vacant & Abandoned Properties



Source: NJGIN, NJDEP, NJOGIS, NJDOT, City of Paterson, Vacant/Abandoned Properties as of 2014

HEYER GRUEL & ASSOCIATES





## OPPORTUNITIES FOR STRATEGIC DEVELOPMENT

### *Vacant/Abandoned Properties List*

Properties that are vacant or abandoned diminish the value of neighborhoods and can become havens for criminal and illicit activity. These properties are a burden on the City and should be aggressively pursued with actions to reduce their blighting impacts. The City's 2014 Master Plan identifies several steps that the City can take to address these properties, including identifying all of the properties that are vacant or abandoned, and aggressively taking code enforcement actions against these property owners.

The City has established an abandoned properties list. There are over 1,000 properties currently listed on the City's Vacant & Abandoned Properties database, which is regularly updated and maintained by the Division of Community Improvements. Many of these properties are within the First Ward Redevelopment Area. The City Council in August 2014 adopted an ordinance (Ordinance #14-034) that requires that owners of properties on this list register their properties, and take actions to indicate they are not abandoning the property, or face stiff penalties. The ordinance also establishes procedures for either the City or another private party to acquire properties that have remained on the list, and are also delinquent in paying property taxes for a period of at least six quarters, through a tax sale. Essentially, if a property remains vacant for an extended period of time, the City can potentially acquire that property and put it to a productive use.

Each of these properties represents an opportunity for the City to transform a blighted property into an asset. There are many properties on the list scattered throughout the First Ward Redevelopment Area, as is shown in the Vacant & Abandoned Properties Map. Most of these properties are small residential lots that are less than 5,000 square feet in area, although there are some are larger parcels. Where multiple adjacent properties are abandoned, they could potentially be acquired together and developed as a larger opportunity site. If the City or a private developer is to acquire any of these properties, they could potentially be used in the following ways:

- Infill housing according to the standards of the zoning districts established by this Plan.
- Mixed Use developments with commercial space on the ground floor and residences or office space above.
- Smaller parcels that may not be suitable for residential or commercial development can be utilized as pocket parks, community gardens, or some other public space.
- Properties located within the designated Open Space District should be targeted for acquisition to be converted to open space.

### *Northside Buyback Program*

The City has received grant funding from the New Jersey Office of Emergency Management, as well as funds from the New Jersey Department of Environmental Protection's Green Acres program, and the CDBG-DR program to finance the purchasing of flood-prone properties along the northern banks of the Passaic River within the First






City of Paterson, NJ

First Ward Redevelopment Plan

## Northside Buy Back Properties

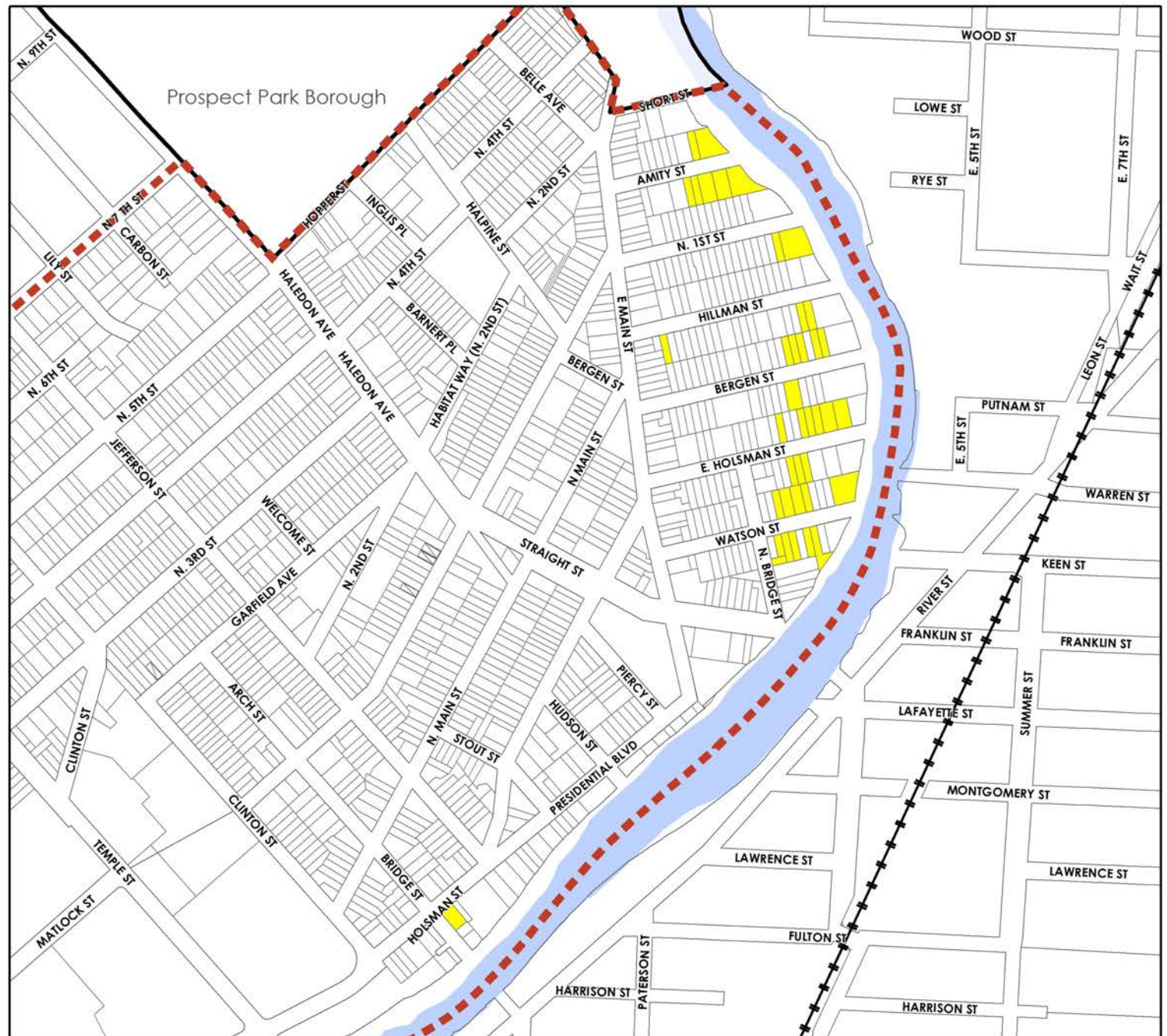
## Northside Buy Back Properties

 Acquired

 First Ward Redevelopment Area


Source: NJGIN, NJDEP, NJOGIS, NJDOT,  
City of Paterson, 2014 MOD IV Data

HEYER GRUEL &amp; ASSOCIATES





Ward. This is the Northside Buyback Program.

The City has been working with a private consulting firm to manage the use of the funds it has received for the Northside Buyback Program. As a part of this contract, the consulting firm may compile a new list of properties to be targeted for acquisition.

In addition to the initial 5.6 million dollars, additional funding through FEMA has been provided to the County to acquire properties in flood-prone areas. A portion of those funds have been earmarked for use in the City of Paterson, and may include properties within the Northside Buyback area.

Through the acquisition of properties along the Passaic River, the City can move forward with the long-term goal of establishing a greenway along the River. However, many more properties will need to be acquired than can be with the limited funds in this program. Many residents and businesses in these areas will need relocation assistance if their properties are to be acquired.

As of the writing of this plan, thirty-nine properties have been acquired, with the most recent having closed in June 2016. Additional funding remain, so continuing to pursue targeted acquisition in this area is an ongoing goal.

#### ***All Public/City Owned Properties***

In addition to sites that the City may acquire either through the Vacant/Abandoned Properties list, or through the Northside Buyback Program, there are a number of assets in the First Ward Redevelopment Area that are already owned by either the City or another public or non-profit agency, such as

the Paterson Housing Authority. While most of the properties in the Area that are owned by a public agency are currently used as schools, public housing, or for other productive purposes, some of these properties can be potentially utilized for more than their current function. The City should consider the potential for mixed uses of their assets in the Area to help meet community needs. For example, public school properties can also serve as community centers when school is not in session.

#### ***General Needs of the Community***

In addition to the greenway along the Passaic River and new housing, there have been several other community facilities and amenities that have been identified as pressing needs within the First Ward Redevelopment Area. These needs have been identified in the Master Plan, Northside Neighborhood Plan, and other plans and initiatives in the Area:

- New First Ward Branch Library – The First Ward Branch Library has been temporarily relocated as the previous building was damaged by flooding. The Library is currently operating out of a small room in the Christopher Hope Center on Temple Street, which does not provide the space or the amenities needed by community residents.
- Northside Community Center – There is a need for a central community resource facility within the First Ward Redevelopment Area. A Community Center that provides many of the social services that residents need, within their neighborhood could be very beneficial to the Area.
- Community Grocery – While there is a



*Public School Recreation Field*



*City Owned Building*



*Flood-Prone Area along Passaic River*





supermarket on West Broadway within the First Ward Redevelopment Area, the northern sections of the Area have limited access to a high quality grocery store that provides fresh foods and produce. The only neighborhood markets serving this section of the Redevelopment Area are small bodega style markets.

- **Public Safety** - This Plan cannot unilaterally provide for improvements in public safety, but the continued implementation of CPTED measures, encouraging the development of additional community facilities and resources, and promoting infill development and rehabilitation of abandoned structures can contribute to the creation of a more safe and secure environment.
- **Green Infrastructure** - The Plan encourages the expansion of the County's Green Streets Pilot program and building on other efforts such as those made at School 5 and School 28. The plan encourages including green infrastructure measures in developments and on public property including additional rain gardens, cisterns, community gardens, and tree planting.

### ***Larger Development Opportunities***

Where the Vacant/Abandoned Properties list, and the Northside Buyback program provide opportunities for infill development of housing or steps towards creating a greenway along the Passaic River, there are a few sites within the First Ward Redevelopment Area that are larger than a standard residential lot, and offer the potential for bigger development projects and connections to the National Historical Park.

These sites include the following properties:

#### ***Block 115 Lot 1***

This 1.24 acre property occupies the entire block between Haledon Avenue, East Main Street, Presidential Boulevard, and Piercy Street. This is the former site of the National Silk Dyeing Company's East Main Street Works, and is listed on the City's Municipal Register of Historic Places. This property is also currently listed on the Vacant/Abandoned properties database.

The property is developed with a former warehouse and factory structure and could be adaptively re-used with a mix of residential and commercial uses. The property is one of two properties located within the A-R Adaptive Reuse District.



***National Silk Dyeing Company***

#### ***Block 201 Lot 2***

This property occupies much of the northern portion of the block between Temple Street, Clinton Street, and Circle Avenue, having frontage on both Circle Avenue and Clinton Street. There is a steep topography in the western end of the property that may pose issues with any potential new development on the site. This property is currently owned by the City, and is developed with a single story structure. This building could potentially be reused to serve a new public purpose, such as a new library facility. The building could also be removed if need be, and a new structure built to serve the needs of the community. The property is located in the Public Use District.



***Block 201 Lot 2***



*Block 801 Lot 8*

This property, which is approximately 3/4 acre in size, is developed with a large historic mill structure, and several smaller industrial buildings. Its location on Totowa Avenue, just a block from Hinchliffe Stadium and only a few blocks from the Great Falls National Historical Park, make it an ideal location for a mixed use facility that takes advantage of its location. These buildings could be reused as loft housing and potentially a restaurant and event space.

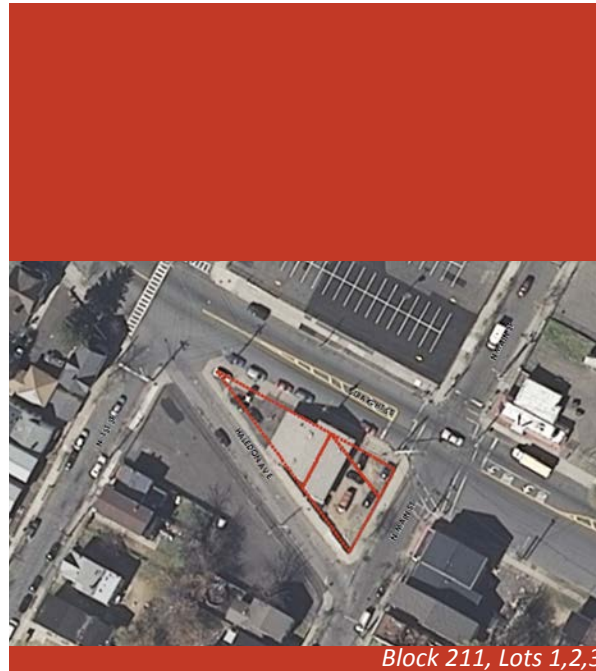
The property is on the City's Municipal Register of Historic Places, as the Miesch Silk Manufacturing Company's Totowa Works. The property is one of the two properties located in the A-R Adaptive

*Miesch Silk Manufacturing*

Reuse District, and should be adapted for use as a mixed use facility.

*Block 211 Lots 1,2,3*

These properties, which are approximately 7,000 square feet in area combined, are currently held in common ownership and developed with an auto service facility. Although relatively small in size, these parcels make up a triangular block along Haledon Avenue between North Main Street and North First Street that is a very prominent site due to the topography and street network surrounding it. This site is one of the most visible properties within the Redevelopment Area. This site could be redeveloped as a new mixed use commercial and residential building that would be the focal point of the neighborhood.

*Block 211, Lots 1,2,3**Ryle Road Gateway*

These properties located along Ryle Avenue and Ryle Road are currently either vacant, or underutilized. They make up the gateway to the Valley of the Rocks and one of the entrances to the National Park. This area could be transformed as an extension of the Passaic River greenway, and include development which would cater to visitors to the National Park. This development could be restaurants, tourist oriented retail, additional recreation opportunities, and even hotels.

The City is in the process of acquiring some of these properties. They are within the National Park Gateway District and it is anticipated that they will help create a connection between the Park, Hinchliffe Stadium, and the neighborhood.

*Ryle Road Gateway Area*

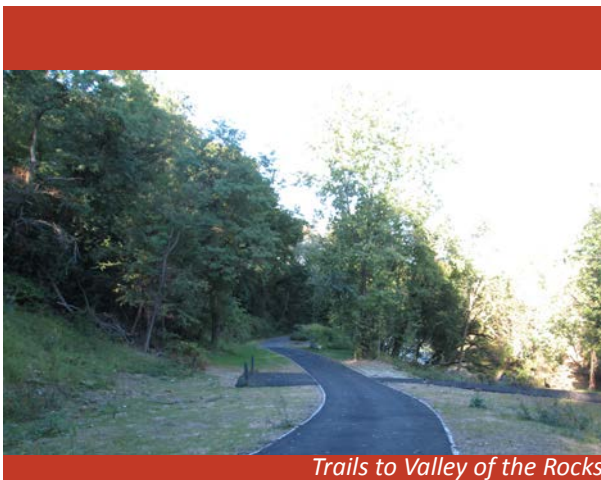


### *Hinchliffe Stadium, Mary Ellen Kramer Park*

Hinchliffe Stadium and Mary Ellen Kramer Park are situated within the National Historical Park boundaries and provide an important overlook and access point to the north side of the Falls. The continued restoration efforts at Hinchliffe Stadium should be accompanied by the development of a new structure that can incorporate a museum, visitor center or other cultural attraction with a restaurant or other similar attraction. This structure should be designed to capitalize on the view of the Falls and the City's skyline, as well as be integrated into the fabric of the Stadium.

The amenities, signage, and other features at Mary Ellen Kramer Park should be upgraded and tied into the development and rehabilitation work at the Stadium site.

The options for additional scenic overlook platforms in and around the Falls should be explored. Glass walkways suspended over portions of the falls could create an unparalleled observation experience.



The properties are privately owned, and are within the C-1 Neighborhood Commercial District.

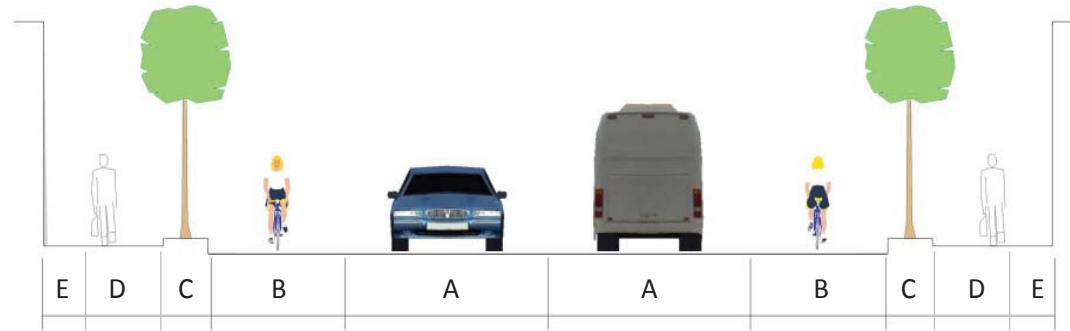
## CIRCULATION IMPROVEMENTS

### Passaic County Complete Streets

Passaic County is proposing a number of improvements to several of the County roads that are within the First Ward Redevelopment Area. The proposed improvements are to transform the roads into Complete Streets. Two types of complete streets are proposed for the roads within the First Ward Area, Downtown Complete Streets, and Regional Complete Streets. The following county roads are proposed to receive Complete Streets treatments by the County:

- Haledon Avenue (CR 504) Downtown Complete Street
- East Main Street (also CR 504) Regional Complete Street
- West Broadway (CR 675) Downtown Complete Street
- Union Avenue (CR 646) Downtown Complete Street
- Belmont Avenue (CR 673) Downtown Complete Street

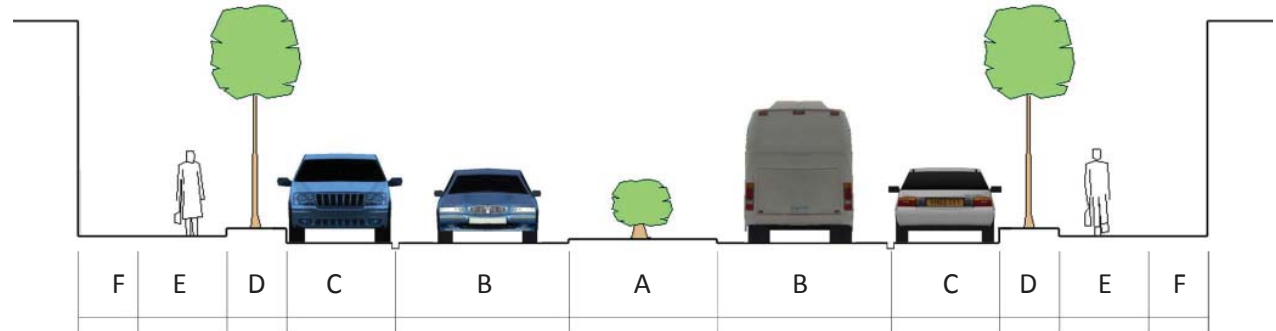
A Downtown Street designed as a complete street would typically include a vehicular travel lane in each direction, which may be separated by a landscaped median, on-street parallel parking on both sides of the street, a 4' wide landscaping strip next to the curb, and relatively wide sidewalks to accommodate high pedestrian traffic. A Regional Complete Street is one that is less focused on providing amenities for the pedestrian



- A. Travel Lane – 10' Minimum, 11' Preferred, 14' Maximum (to accommodate trucks and buses)
- B. Shoulder or Dedicated Bicycle Lane – 8' Preferred (6' Minimum for Dedicated Bicycle Lane)
- C. Curb Zone – 4' Minimum, 8' to 12' Preferred
- D. Sidewalk Zone – 4' Minimum, 5' to 8' Preferred
- E. Frontage Zone – 2' Minimum for Residential, 4' Minimum Preferred for Commercial

*Passaic County Regional Street, Courtesy of Passaic County Department of Planning*

*Passaic County Downtown Street, Courtesy of Passaic County Department of Planning*



- A. Median – 6' Minimum for Pedestrians, 10' Minimum for planted, 16' Minimum for turning lanes
- B. Travel Lane – Auto Only: 10' Minimum; 11' Preferred (to be shared with bicycles); 12' Maximum (to accommodate high volume of bus traffic)
- C. Parking – 8' Minimum; 4' Buffer area if dedicated bicycle lanes installed adjacent to parking to accommodate "Door Zone"
- D. Curb Zone – 4' Minimum/6' to 8' Preferred
- E. Sidewalk Zone – 5' Minimum/6' to 8' Preferred
- F. Frontage Zone – 2' Minimum for Residential, 4' to 8' Preferred for Commercial



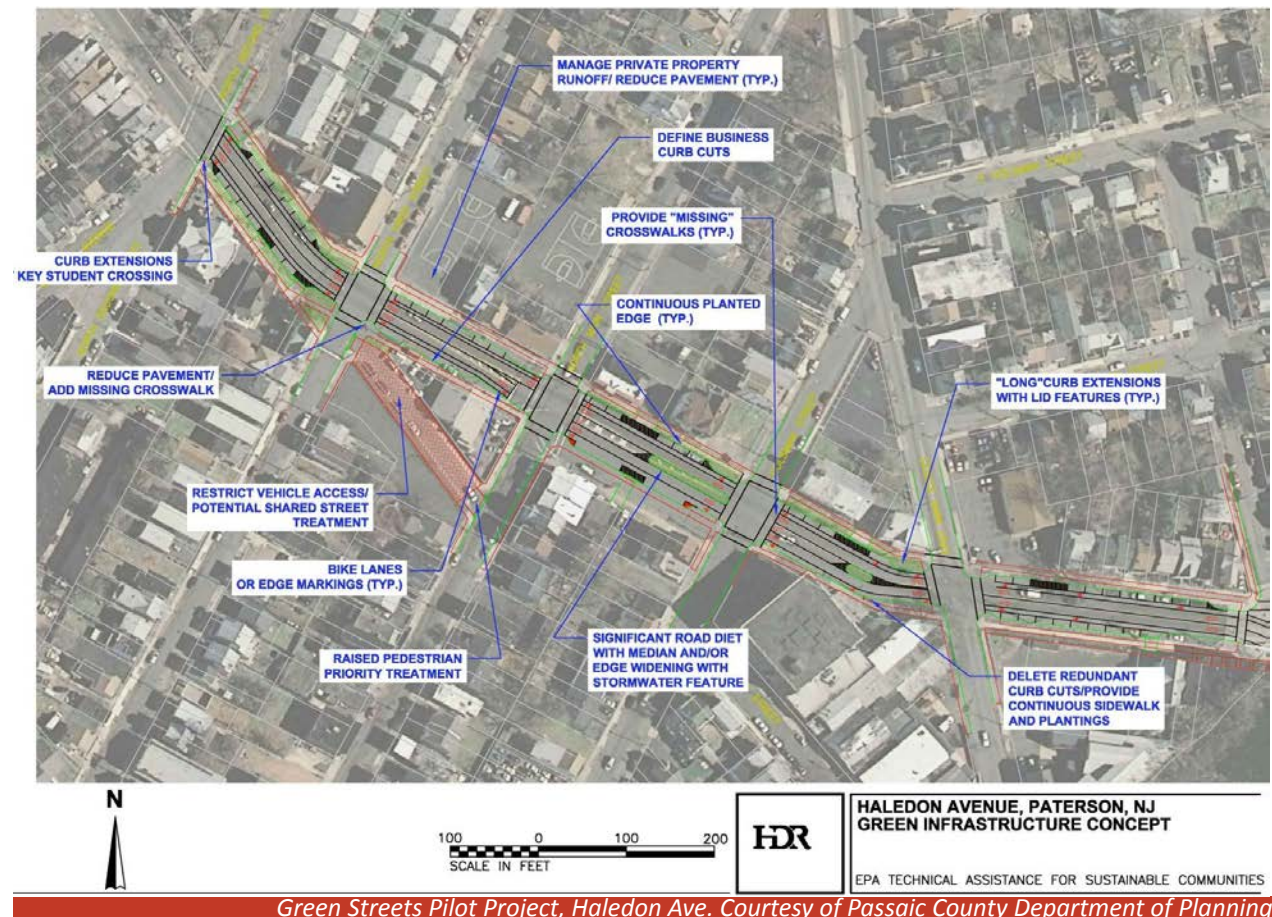
environment, and more focused on efficiently moving vehicular traffic, having relatively wider travel lanes and no on-street parking.

These roads, which are the primary transportation thoroughfares in the Area, and under the jurisdiction of the County, if improved in the ways described in the Passaic County Master Plan Transportation Element can help to revitalize the First Ward Redevelopment Area. Complete streets along the primary commercial corridors will help provide access to and from businesses for local residents, as well as people beyond the boundaries of the Area.

### ***Passaic County Green Streets***

Along with a Complete Streets Approach to utilizing public Rights-of-Way for all potential users, the County is also pursuing a green streets initiative. A green street is one that incorporates low-impact design elements, and attempts to manage stormwater through natural biological processes, rather than simply channeling water through a sewer system.

The County has proposed that Haledon Avenue in Paterson serve as a pilot project of the Green Streets Initiative. From North 2nd Street to the bridge at North Bridge Street, Haledon Avenue is proposed to have a number of 'green streets' improvements that will work in conjunction with the proposed Complete Streets improvements. These improvements include landscaped curb extensions, reductions in the number of existing curb cuts, continuous planted edges, and new crosswalks. The diagram of the pilot project from the County illustrates the proposed improvements.



*Green Streets Pilot Project, Haledon Ave. Courtesy of Passaic County Department of Planning*

These proposed street improvements can help to make this area of the City not only more sustainable, but a more attractive streetscape as well.

### **PROPOSED CIRCULATION IMPROVEMENTS**

The proposed Green Streets and Complete Streets improvements on several of the County roads throughout the Area can significantly benefit the communities of the First Ward if they are implemented. However, this still may leave some gaps in the circulation network where similar improvements can be made. The County routes that are proposed as Complete Streets are generally the north/south thoroughfares on either the east side or the west side of the Area. These roads connect to areas such as the Downtown of the City, and municipalities to the north, but not necessarily with each other. There are several other streets which if given similar treatment as Complete Streets and/or Green Streets could help facilitate connectivity throughout the Area. Making minor improvements to these streets to include more adequate provisions for bicycles, pedestrians, and buses would connect the entire network more efficiently:

- Presidential Boulevard – An east/west thoroughfare along the Passaic River, improvements on this street, which is a County Road, could help connect West Broadway to East Main Street. This could potentially include designated bicycle lanes, improved bus stops with shelters, and green streets to beautify the streetscape.
- North 7th Street – Another street north of Presidential Boulevard should be targeted for improvements as well. Whether it be North

3rd, 4th, or 5th, or 7th Street, this could provide better connections between Haledon Avenue and Belmont Avenue.

In addition to facilitating connectivity within and around the First Ward Redevelopment Area, connections to the Great Falls National Historical Park must be strengthened. The Great Falls and the National Historical Park that surrounds them are a vital piece of the history, and future revitalization of the entire City. The Great Falls National Historical Park borders the First Ward Redevelopment Area just south of Totowa Avenue and Hinchliffe Stadium. In order to facilitate connections to Great Falls, the following steps should be taken:

- Maple Street should be made into a Green Street/Complete Street. Maple Street leads directly to the National Historical Park and runs alongside Hinchliffe Stadium. Making Maple Street more bicycle and pedestrian friendly, as well as more attractive in general would help to get people to and from the park. This would also encourage visitors to the park, to explore other areas of the City.
- Totowa Avenue should also be targeted for similar improvements, as it can serve as an east/west connector between Maple Street, and West Broadway. Bicycle and pedestrian improvements on both Totowa Avenue and Maple Street could then connect to the existing bicycle/pedestrian path along the northern banks of the Passaic River near Ryle Road in the Valley of the Rocks.
- Improved directional signage should be installed to direct vehicles and pedestrians from primary thoroughfares such as West



*Dedicated Bicycle Lane, Philadelphia PA*







*Curb Extension and Crosswalk, Summit NJ*



City of Paterson, NJ

First Ward Redevelopment Plan

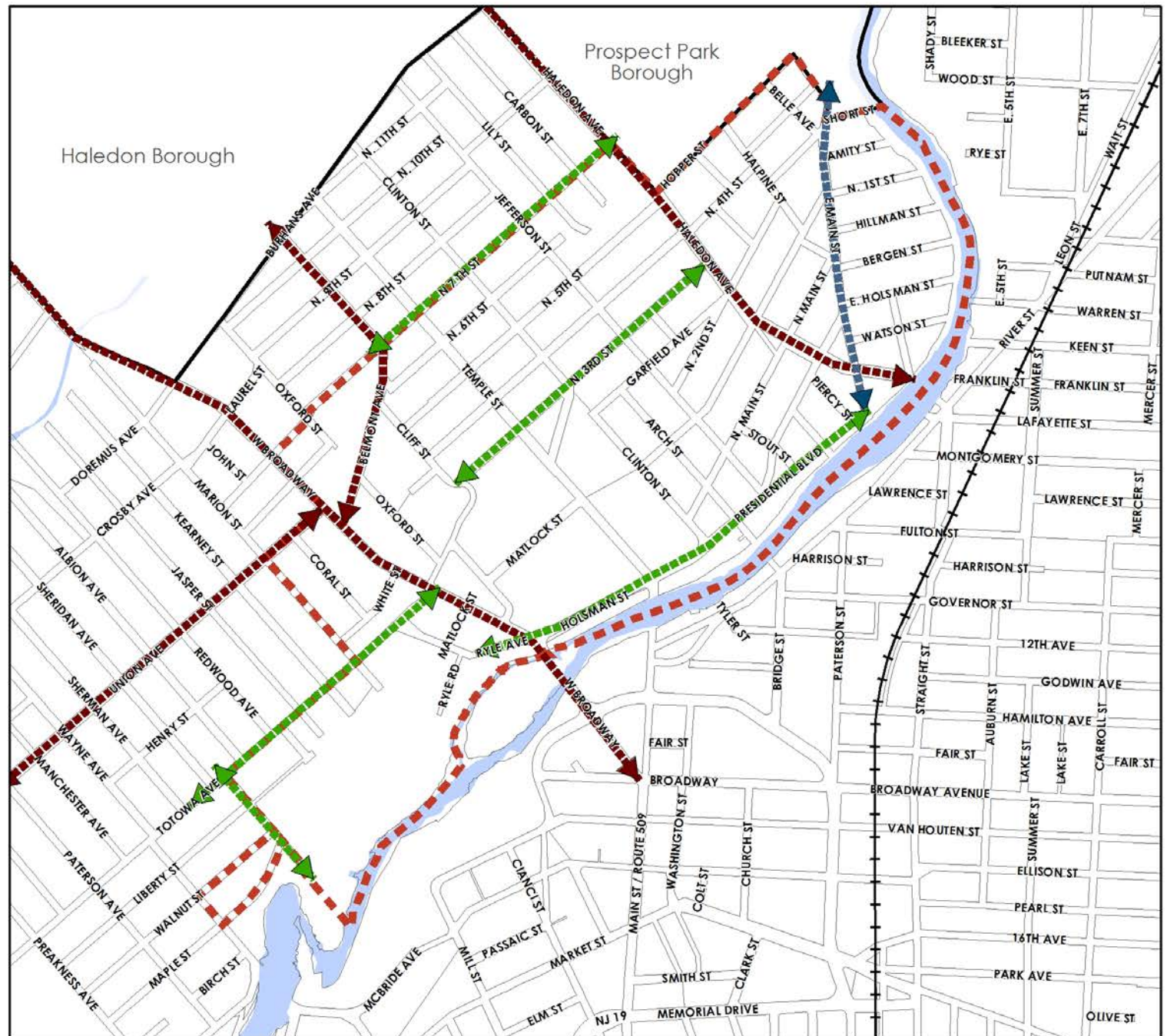
## Proposed Circulation Improvements

-  County Regional Streets
-  County Downtown Streets
-  Proposed Complete/Green Streets
-  First Ward Redevelopment Area



Source: NJGIN, NJDEP, NJOGIS, NJDOT,  
City of Paterson

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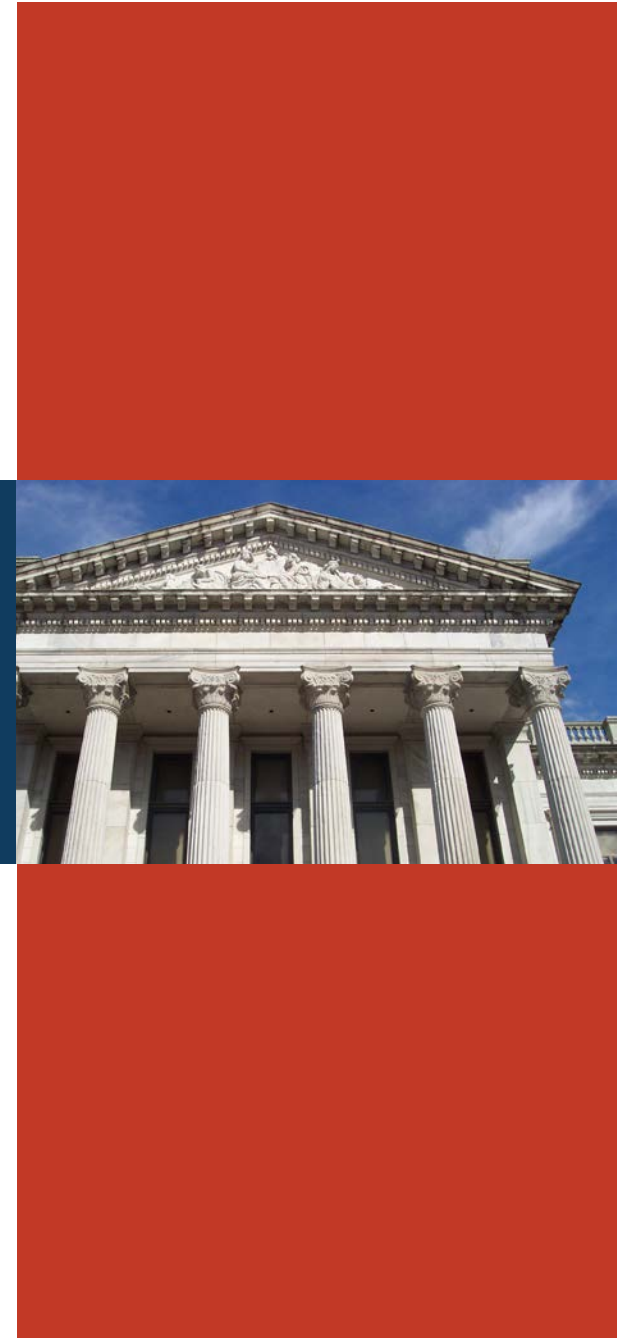






# RELATIONSHIP TO OTHER PLANS

## PATERSON FIRST WARD







## RELATIONSHIP TO OTHER PLANS

This updated Redevelopment Plan has been prepared as the direct result of the City of Paterson's Master Planning Process. As a result, many of the goals and objectives of this Plan are in direct compliance with the City, County, and State Planning documents. The following is a brief overview of the appropriate planning documents of regional significance, and how this Plan relates to those documents.

## 2014 PATERSON MASTER PLAN

The Planning Board of the City of Paterson adopted a new Master Plan in April of 2014 which provides a comprehensive guide for the development of the City. The Master Plan includes several goals and objectives that have relevance to the Redevelopment of the First Ward Area.

### Land Use Goals

- To address the problems associated with vacant and abandoned properties
  - Convert vacant and abandoned properties to occupied properties on the tax rolls
  - Encourage adaptive reuse of obsolete buildings
- To increase open space and recreation in the City without diluting the tax base
- To promote infill housing development
- To improve the viability of the City's neighborhood commercial corridors
- To address outdated or expired redevelopment plans
- To increase resiliency to storm and flood

events

- Acquire properties and remove businesses and residents from highly flood-prone areas
- To develop targeted opportunity sites that will serve as catalysts and symbols of the revitalization of the City of Paterson

### Urban Design Goals

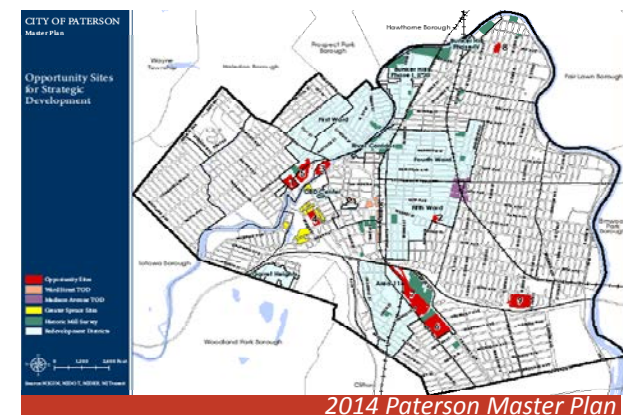
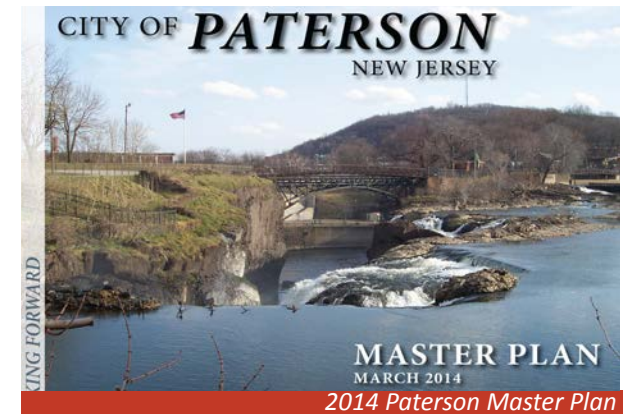
- To promote and implement appropriate and attractive design standards for new construction
- To improve the appearance of the City's streets
- To enhance City gateways as "green streets" with green infrastructure

### Housing Goals

- To increase the supply of affordable housing
  - Provide affordable housing options amongst mixed income neighborhoods
- To provide opportunities for the construction and/or rehabilitation of affordably priced market rate housing
  - Consider conversion of vacant industrial buildings to residences
  - Promote infill housing development on vacant properties
- To stabilize and improve existing residential neighborhoods

### Community Facilities Goals

- To provide facilities to meet the needs of

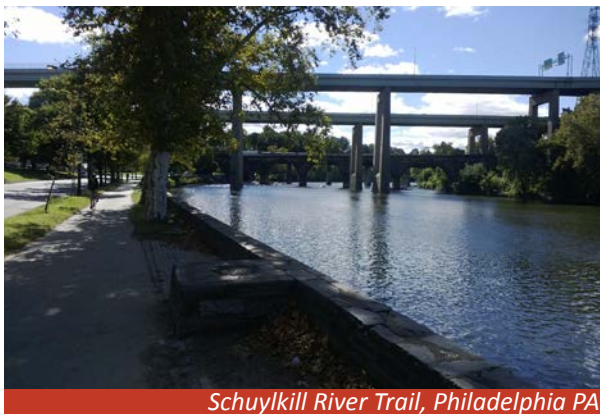




*Abandoned Properties*



*Great Falls National Historical Park*



*Schuylkill River Trail, Philadelphia PA*

- residents of all ages, in each neighborhood
- Address the need for libraries or community resource centers

### ***Economic Development Goals***

- To capitalize on the recently designated Great Falls National Historical Park
  - Increase business activity around the Park
  - Renovate historic facilities to create attractions for visitors
- To promote the development of vacant and abandoned properties
- To provide opportunities for local employment growth

### ***Historic Preservation Goals***

- To encourage the restoration and/or adaptive reuse of historic structures in the City
  - Historic Mills
  - Hinchliffe Stadium

### ***Recreation/Open Space Goals***

- To provide increased access to open space for City residents
- To provide more opportunities for active and passive recreation throughout the City

The Master Plan also made several specific recommendations regarding the First Ward Redevelopment Area:

- Acquire properties along the Passaic River and convert the riverfront into a greenway/public park
- Promote development of a variety of housing options in each neighborhood
- Promote neighborhood commercial uses in appropriate locations such as Bridge Street/Haledon Avenue and West Broadway
- Update the First Ward Redevelopment Plan to include the proposals from the Northside Neighborhood Plan, including:
  - Establish a riverwalk along the Passaic River
  - Consider relocation strategies for businesses in the flood zone to be moved to Bunker Hill
  - Implement green infrastructure solutions to help manage flooding
  - Establish community gardens
  - Remove blighted buildings and structures, and replace them with affordable housing
  - Provide library services in the neighborhood
- Facilitate connections to the Great Falls National Historical Park from other prominent locations in the City



## GREAT FALLS NATIONAL HISTORICAL PARK General Management Plan

The National Park Service of the Department of the Interior has recently finalized the General Management Plan (GMP) for the Paterson Great Falls National Historical Park. The overall goal of the GMP is to attract tourism to the Park through transformation of the Park into an industrial heritage historical area, and providing opportunities for outdoor passive recreation, capitalizing on the Falls two greatest resources.

The GMP for the Park creates three alternative development scenarios:

- Alternative A is a no action scenario, where only maintenance of existing facilities takes place.
- Alternative B focuses on landscape exploration. This scenario includes recreation areas to explore the natural features of the Falls, and expanded visitor facilities.
- Alternative C focuses on the industrial heritage of the area. This scenario focuses on educational opportunities through rehabilitating historic mills and bringing to life the Industrial Revolution which has its roots in Paterson.

The implementation of the GMP is to include portions of both Alternatives B and C, in some combination.

Portions of the National Park, namely Hinchliffe Stadium and the Valley of the Rocks, overlap with the boundaries of this Redevelopment Plan. The GMP calls for the revitalization and reuse of

Hinchliffe Stadium, and for the Valley of the Rocks to incorporate new outdoor recreation opportunities and river access, such as walking trails, outdoor classrooms, and seating areas with views of the river and Great Falls.

This Plan is consistent with the goals of the General Management Plan for the National Park.

## STATE PLANS

### State Development and Redevelopment Plan (SDRP)

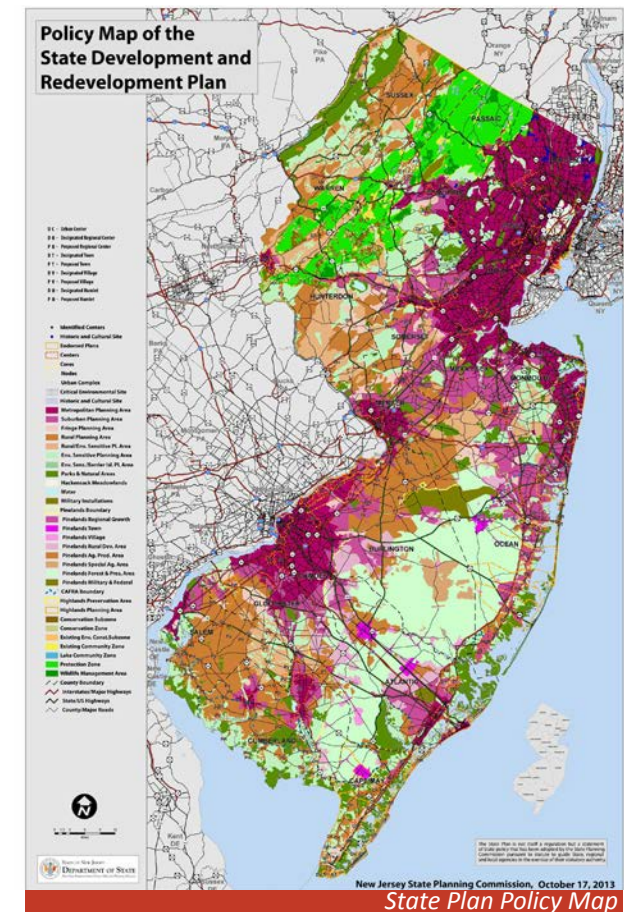
In 2001, the State Planning Commission adopted the second State Development and Redevelopment Plan (SDRP). The SDRP is a document that, while not binding, guides State-level development and redevelopment policy as well as local and regional planning efforts. The Plan includes eight statewide goals and dozens of policies which are intended to implement the goals. The goals are as follows:

1. Revitalize the State's cities and towns.
2. Conserve the State's natural resources and systems.
3. Promote beneficial economic growth, development and renewal for all New Jersey residents.
4. Protect the environment, prevent and clean up pollution.
5. Provide adequate public facilities and services at a reasonable cost.
6. Provide adequate housing at a reasonable cost.
7. Preserve and enhance areas with historic, cultural, scenic, open space, and recreational

value.

8. Ensure sound and integrated planning and implementation statewide.

The SDRP also includes a State Plan Policy Map, which divides the state into regions known as Planning Areas. The Map also identifies 'centers', locations into which development is to be directed. The City of Paterson is located within Planning Area 1, or the Metropolitan Planning Area, and is designated as one of 8 urban centers of the SDRP.





This Plan meets stated goals 1, 6, and 7 in particular of the SDRP.

- The intended purpose of this Plan is to revitalize certain portions of the City of Paterson, a recognized urban center by the SDRP.
- New and improved housing is a central aim of this Plan.
- Through the development of a greenbelt or riverwalk around the Passaic River, access to neighborhood parks and open space will be increased for area residents.

### ***State Strategic Plan***

The final draft of the State Strategic Plan was last released in 2011. While the State Strategic Plan has not been officially adopted, and the SDRP is still the official State Plan, it is still prudent to review the relationship between this Plan and the State Strategic Plan Draft. This Plan is consistent with the stated goals of the State Strategic Plan. Specifically this Plan exemplifies the guiding principle of “Spatial Efficiency”, where new development is encouraged to be located in areas where infrastructure is already in place to support growth.

This Plan meets the following goals of the State Strategic Plan:

- Targeted economic growth of industries of statewide and regional importance, with healthcare being one of the targeted industries.
- Major urban centers are to be targeted as priority growth investment areas

- Garden State Value #1 Concentrate development and mix uses
- Garden State Value #2 Prioritize Redevelopment, infill, and existing infrastructure
- Garden State Value #3 Increase job and business opportunities in priority growth investment areas

### ***PASSAIC COUNTY PLANS***

#### ***Passaic County Master Plan Land Use Element***

The County Master Plan was last updated in April 1988 when the Land Use Element was added. The following are some of the goals of the “Land Use Plan”, that are in tune with the goals and objectives of this Redevelopment plan:

- **Balanced Development:** Provide for balanced development of the County with various types of residential uses, and with non-residential uses to support them; to encourage and maintain a balance between jobs and housing so that workers can find suitable housing within the county.
- **Concentrate Development and Conserve Critical Lands:** Concentrate development in areas with existing infrastructure in order to promote efficient transportation, conserve energy and reduce pollution; Encourage the location of new facilities; Encourage the location of new facilities and services near housing and jobs.
- **Business and Industry:** Provide for commercial and industrial areas sufficient to maintain a viable economic base for the County.

#### ***Passaic County Master Plan Transportation Element***

A primary focus of the Transportation Element of the County’s Master Plan is on implementing “complete streets” on many county thoroughfares. A complete street is a street that has specific improvements intended to provide for the safe and efficient transportation of many differing users using multiple different modes of transportation. This would include provisions for automobiles, bicycles, pedestrians, buses, and wheelchairs, all potentially using the same public right-of-way as a safe and suitable means of transportation.

The County Plan identifies several different classifications of complete streets, two of which are proposed for County roads within the First Ward Redevelopment Area. A Regional Street is a major travel route that handles the highest volume of traffic on County roadways. A typical Regional street with the complete streets treatment will have vehicular travel lanes in each direction, and will also have a 6’ – 8’ wide shoulder or dedicated bicycle lane on each side of the street. A small landscaping strip of at least 4’ will be located along the curb, and a minimum of 4’ of sidewalk width is also recommended to be provided.

A Downtown Street according to the Passaic County Transportation Master Plan is one in which the land uses surrounding the street are generally mixed use, and serve central business districts or other small scale business districts. A Downtown Street designed as a complete street would typically include a vehicular travel lane in each direction, which may be separated by a landscaped median, on-street parallel parking on both sides of the

street, a 4' wide landscaping strip next to the curb, and relatively wide sidewalks to accommodate high pedestrian traffic.

The following county roads are proposed to receive "complete streets" treatments by the County:

- Haledon Avenue (CR 504) Downtown Complete Street
- East Main Street (also CR 504) Regional Complete Street
- West Broadway (CR 675) Downtown Complete Street
- Union Avenue (CR 646) Downtown Complete Street
- Belmont Avenue (CR 673) Downtown Complete Street

#### ***Passaic County Master Plan Parks, Recreation, and Open Space Comprehensive Plan Element***

The County's Parks and Open Space Plan Element was recently adopted in July of 2014, and includes a new vision for County operated parks, and several goals and objectives that have relevance to the First Ward Redevelopment Area of the City of Paterson:

- Expand the Passaic County Park System
  - Acquire new properties in strategic locations, such as along rivers, within historic and scenic byways, in areas that are adjacent to existing areas of the Passaic County Park System, and in historically underserved municipalities.
  - Partner with municipal, state, and

local government, as well as private organizations to acquire and maintain new properties.

- The Element recommends acquiring properties along the Passaic River, and providing increased access to water based recreational activities.
- Block 801 Lots 5 and 6 (Totowa Avenue) are listed as sites that are targeted to be acquired by the County for additional open space and recreation opportunities

#### ***Passaic County Future, the Passaic County Sustainability Plan***

In May of 2013, the County adopted a Sustainability Element of their Master Plan. The Sustainability Element is a comprehensive approach to achieving sustainability in Passaic County, and includes a number of recommendations that are consistent with the aims of this Plan.

- Create blue-ways and enhanced flood-plains in flood prone properties.
- Encourage mixed use, and live/work development and redevelopment.
- Improve sidewalk conditions and street crossing amenities along existing or potential pedestrian corridors.
- Encourage new housing options through adaptive reuse and infill development.
- Increase access to and availability of developed green space for active and passive recreation.

### ***PLANS OF ADJACENT MUNICIPALITIES***

#### ***The Master Plan for the Borough of Prospect Park***

The City of Paterson shares a border with eight other municipalities in the State of New Jersey. However, only the Borough of Prospect Park to the north of the City is adjacent to the First Ward Redevelopment Area. Prospect Park shares a border with Paterson along Haledon Avenue and Hopper Street, in the northern end of the Area. The rest of the Redevelopment Area is entirely contained within other neighborhoods of the City of Paterson. Prospect Park last updated its land use plan in 2009. Both Haledon Avenue and Hopper Street in Prospect Park are designated for a combination of residential and neighborhood commercial uses, similar to the zoning designations in Paterson. The land use between the two municipalities is compatible and consistent.



# OTHER STATUTORY REQUIREMENTS

## PATERSON FIRST WARD





## **OTHER STATUTORY REQUIREMENTS**

### ***Property to Be Acquired***

No direct property acquisition through the use of eminent domain is authorized or anticipated as a part of this Plan.

The primary means of property acquisition for the City will be through the Vacant and Abandoned Properties List, and the authority possessed by the City for acquiring properties that have been determined to be abandoned through the procedures outlined in Ordinance # 14-034.

### ***Relocation Assistance***

No private property is proposed to be acquired through the use of eminent domain as a result of this Plan. No relocation assistance will be necessary.

### ***Affordable Housing***

No affordable housing units are identified to be removed as part of the implementation of this Redevelopment Plan. Any affordable housing obligation incurred by a redeveloper shall be addressed through a Redeveloper Agreement.







# IMPLEMENTATION

## PATERSON FIRST WARD



## **ADMINISTRATIVE PROCEDURES**

### ***Redevelopment Entity***

The City Council shall serve as the Redevelopment Entity.

### ***Amending the Redevelopment Plan***

Upon compliance with the requirements of applicable law, the City Council of Paterson may amend, revise or modify this Redevelopment Plan, as circumstances may make such changes appropriate.

### ***Duration of the Redevelopment Plan***

This Redevelopment Plan, as amended, shall be in full force and effect for a period of thirty (30) years from the date of approval of this Plan by the City Council.

### ***Conveyance of Land***

The City Council may sell, lease, or otherwise convey to a redeveloper for redevelopment, subject to the restrictions, controls and requirements of this Redevelopment Plan, all or any portion of the land within the Redevelopment Area which becomes available to disposal by the municipality.

### ***Redevelopment Agreements***

The following restrictions and controls on redevelopment are statutorily imposed by the Local Redevelopment and Housing Law, in connection with the selection of a redeveloper or redevelopers and shall apply notwithstanding the provisions of any zoning or building ordinance or

other regulations now or hereafter in force. The restrictions and restraints shall be implemented by appropriate covenants or other provisions in redeveloper agreements and/or disposition instruments.

- The redeveloper(s) will be obligated to carry out the specified improvements in accordance with the Redevelopment Area Plan.
- The redeveloper(s), its successors or assigns shall devote land within the Redevelopment Area to the uses specified in this Redevelopment Plan.
- The redeveloper(s) shall begin the development of said land for the use(s) required in this Redevelopment Plan within a period of time that the City Council fixes as reasonable.
- Until the redeveloper(s) completes construction of the improvements, the redeveloper(s) will not be permitted to sell, lease, or otherwise transfer or dispose of property within the Redevelopment Area without prior written consent of the City.
- Upon completion of the required improvements, the conditions determined to exist at the time the Redevelopment Area was determined to be in need of redevelopment shall be deemed to no longer exist, and the land and improvements thereon shall no longer be subject to eminent domain as a result of those determinations.
- No covenant, agreement, lease, conveyance or other instrument shall be effected or executed by the redeveloper(s), the City Council, or the successors, lessees, or assigns of either of them, by which land in the Redevelopment Area is restricted as to



sale, lease, or occupancy upon the basis of race, color, creed, religion, ancestry, national origin, sex or marital status.

- Neither the redeveloper(s) nor the City Council, nor the successors, lessees, or assigns shall discriminate upon the basis of race, creed, religion, ancestry, national origin, sex or marital status in the sale, lease or rental or in the use and occupancy of land or improvements erected or to be erected thereon, or any part therein.



## FUNDING OPPORTUNITIES

The redevelopment of the First Ward Area will require a mixture of private and public funding. Areas within adopted Redevelopment Plans qualify for several sources of government funding. These funds should be used to leverage private investment. The following is a listing of the most relevant current potential sources of funding for community improvements along with a brief description of those programs. The City already participates in many of these programs. This list is not intended to be exhaustive. There are numerous other sources of funding available and new options may become available in the future that should be considered.

### Federal Programs

- Community Development Block Grants (CBDG) – Community Development Block Grants are a department of Housing and Urban Development (HUD) program that provides a range of housing and community development activities, but must meet one of the program’s objectives of benefiting low and moderate income families, preventing or eliminating blight, or addressing emergency needs.
- HOPE VI Revitalization Grants – HOPE VI is a HUD funded grant that can be used to rehabilitate or transform distressed public housing developments. Funds are given to projects that will help to enable self-sufficiency in public housing residents and de-concentrate urban poverty. The new mixed income housing project at the former Alexander Hamilton Homes site was funded

through a HOPE VI grant.

- Section 8 Voucher Program – Another HUD program that is administered through the Paterson Housing Authority, Section 8 vouchers provide subsidies for low income families to be used for direct housing rental assistance.
- Home Investments Partnership Program (HOME) – The HOME program provides grants to communities specifically for the development of affordable housing for low income households. HOME funds are awarded annually as grants to participating municipalities, in the form of trust funds.
- USEPA Brownfields grants – The Environmental Protection Agency provides several grant and revolving fund loan programs to aid in the assessment and clean-up of brownfield sites.

### State Programs

- NJEDA Economic Redevelopment and Growth Grant (ERG) – The ERG program provides financial assistance to help with gap financing for development projects which have secured traditional debt and equity financing, but fall short of total development costs, creating a small financing gap. Tax credits or grant reimbursements are available for up to 20% of total development costs for approved projects. Projects in Paterson may qualify for additional funding, up to 40% due to the City being identified as a distressed municipality in a growth zone.
- NJEDA Grow NJ program – This program, which provides financial incentives for companies to relocate and expand in New

Jersey. The City of Paterson is a ‘qualified incentive area’ and therefore is able to receive companies using the funds from this program. The program provides both grants and insurance tax credits for creating or retaining jobs.

- NJEDA Fund for Community Economic Development – This program provides loans to other institutions to fund small business loans and economic development initiatives in urban communities such as Paterson. Direct loans for finance gaps in development projects are also available through this program.
- NJEDA Angel Investor Program – The Angel Investor Program provides tax credits of up to 10% of a qualified investment up to \$500,000 for investments in technology businesses that employ less than 225 persons.
- NJEDA Brownfields and Contaminated Site Remediation Program – The NJEDA Brownfields program provides funding for brownfields remediation through new sales taxes that are derived from new businesses located on a former brownfield site. Developers can qualify for up to be reimbursed for up to 75% of the costs of remediation efforts of the site.
- NJDEP Green Acres – The Green Acres program uses State funds to provide matching grants to non-profit organizations, and grants and loans to municipalities to fund the acquisition of property to be used as outdoor recreation parks or conserved open space. This includes athletic fields, running trails, and playgrounds.
- NJDEP Blue Acres – The Blue Acres program is a subsection of the Green Acres program

that specifically targets lands that are located in the floodways of the Delaware, Passaic, or Raritan Rivers. Properties that are prone to damage by flooding are eligible for acquisition through the Blue Acres program.

- New Jersey Environmental Infrastructure Financing Program – NJEIT provides a variety of financing and loan options for many different environmental infrastructure projects, such as stormwater management, combined sewer remediation, wastewater treatment, brownfields remediation, and projects to address clean drinking water standards.
- NJDOT Bikeways – The NJ Department of Transportation provides grants to counties and municipalities to create new bicycle infrastructure to promote bicycling as a viable form of transportation. Bicycle paths and dedicated bicycle lane projects are given priority funding.
- NJDOT Safe Streets to Transit (SSTT) – This is a project by the NJDOT to assist municipalities in increasing accessibility of public transportation services. Grants may be provided for projects such as intersection safety improvements, new sidewalks, traffic control devices, and pedestrian signals that are within 1 mile of a mass transit stop.
- NJDOT Local Bridges Future Needs – This program provides state funding to help improve bridge structures that are under County jurisdiction. Funds can be used for rehabilitation, preventative maintenance, or replacement of structurally deficient or functionally obsolete bridges.
- NJHMFA Live Where You Work – This is a program that provides assistance to first time

home buyers and others in eligible urban target areas who wish to purchase a home and live in the town where they work.

#### ***Passaic County Programs***

- Open Space and Farmland Preservation Trust Fund (OSFPT) – The County's OSFPT provides funds to municipalities or non-profit conservation agencies to acquire or preserve land or water areas that have a natural, scenic, or open condition. Lands acquired or preserved can be used for recreation purposes, or as preserved natural environments. Funds for the OSFPT come from an additional property tax that is levied against all properties in the County to provide a general fund.
- Passaic County Improvement Authority (PCIA) Financing Programs – PCIA provides redevelopment bonds for projects, and financing options for many of the tax credit programs offered by the State. PCIA also offers municipal and non-profit financing programs for capital projects and other improvements, including schools.